

2 Station Road, Borehamwood

Design and Access Statement - July 2019



WHITE
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1.0 Introduction

This design and access statement is for the proposed demolition of the existing dilapidated cottage on the site and the construction of 9 new apartments at 2 Station Road, Borehamwood, WD6 1DF.

The submitted proposal builds on the Pre-application advice (18/0193/PA) received on the 29th January 2019.

This report should be read in conjunction with the following separate reports and drawings:

Reports:

- Ground investigation report.
- GPR underground utilities survey.
- TTP Transport report
- Preapp feedback letter - '2 Station Rd 180193PA letter 20190129'

Existing drawings:

- SW302 - 01 - Location Plan
- SW302 - 10 - Existing Block Plan
- SW302 - 13 - Existing Elevations

Proposed drawings:

- 5286_00_001 - Site plan
- 5286_00_100 - Ground floor plan
- 5286_00_102 - First floor plan
- 5286_00_103 - Second floor plan
- 5286_00_104 - Roof plan
- 5286_00_200 - Proposed elevations



2.0 Site and Context

Site Location

The site is located in a residential area. Only a short walk from Elstree and Borehamwood train station and town centre. The site is therefore well serviced for transport and town centre public amenities. There are significant residential developments under construction immediately to the North West of the site.

Vehicular access is via Coleridge Way to the South of the site and there is a cycle access connecting to the area only a short distance away. The nearest park, 300m to the East is Martin's walk.

Site boundaries:

- To the North, wire mesh boundary fence to old Gasworks site, currently in residential development as per Planning Application 16/1932/FUL.
- To the South, Low concrete wall to the pavement by Coleridge Way.
- To the East, low timber fence to pedestrian way connecting to Shelley Close.
- To the West, timber fence between the garden and the pedestrian & cycle route. There is another timber fence between this route and the adjacent Oakwood House development.



2.0 Site and Context



View from Station Road of new developments looking south



View from Station Road looking south with site on left hand side.



View of site from South West.



View of site looking North from Coleridge Way showing existing cottage.



View of existing garages on the site



View looking North along path to the East of the site.

2.0 Site and Context

Archaeological Assessment

Land at Station Road, Borehamwood, Hertfordshire has been previously assessed for its below ground archaeological potential. In accordance with central, regional and local government planning policy and guidance, desk-based study has clarified the archaeological potential of the previous Gasworks including 2 Station Road.

In terms of relevant designated heritage assets, no World Heritage sites, Scheduled Ancient Monuments, Historic Battlefield or Historic Wreck sites are identified within the study site or its immediate vicinity. The site does not lie within an identified area of archaeological potential.

The Gas works site was considered likely to have a generally low archaeological potential for all past periods of human activity.

Based on the available information, further archaeological mitigation measures were not recommended.

Previous Uses

- Originally part of the Gas works site on Station Road and now decommissioned. The larger part of this site is currently being developed for residential apartments.
- The remaining land/building at 2 Station Road has had various uses but is now derelict and deteriorating.

Ground Investigation Results

An independent Ground Investigation (Geo-Environmental & Geotechnical Assessment) was carried out by Jomas Associates Ltd. The report of this study (included in Appendix B) refer to a soil contamination of dibenzo(ah)anthracene in one of the samples taken.

This report also includes directions on how to deal with this issue (8.1.1, paragraph 3).

We will incorporate advice from Thames Water on an acceptable proposal for retaining (and repairing if needed, see Appendix B 8.1.1, paragraph 3) the existing underground culvert which runs underground close to the north boundary of the property, which on initial investigation will cause no disruption to this utility.

2.0 Site and Context

Flood risk

After consultation with the Environment Agency, It has been confirmed that 2 Station Road is not in Flood Zones 2 or 3 but in Zone 1. See link below.

<https://www.gov.uk/guidance/flood-risk-assessment-for-planning-applications#when-you-need-an-assessment>

The plot is partially in a Low, partially in a Very Low Flood risk area, when considering surface water. See link below.

<https://flood-warning-information.service.gov.uk/long-term-flood-risk-map?eastng=519260.91&northing=196122.66&address=10034167044&map=SurfaceWater>

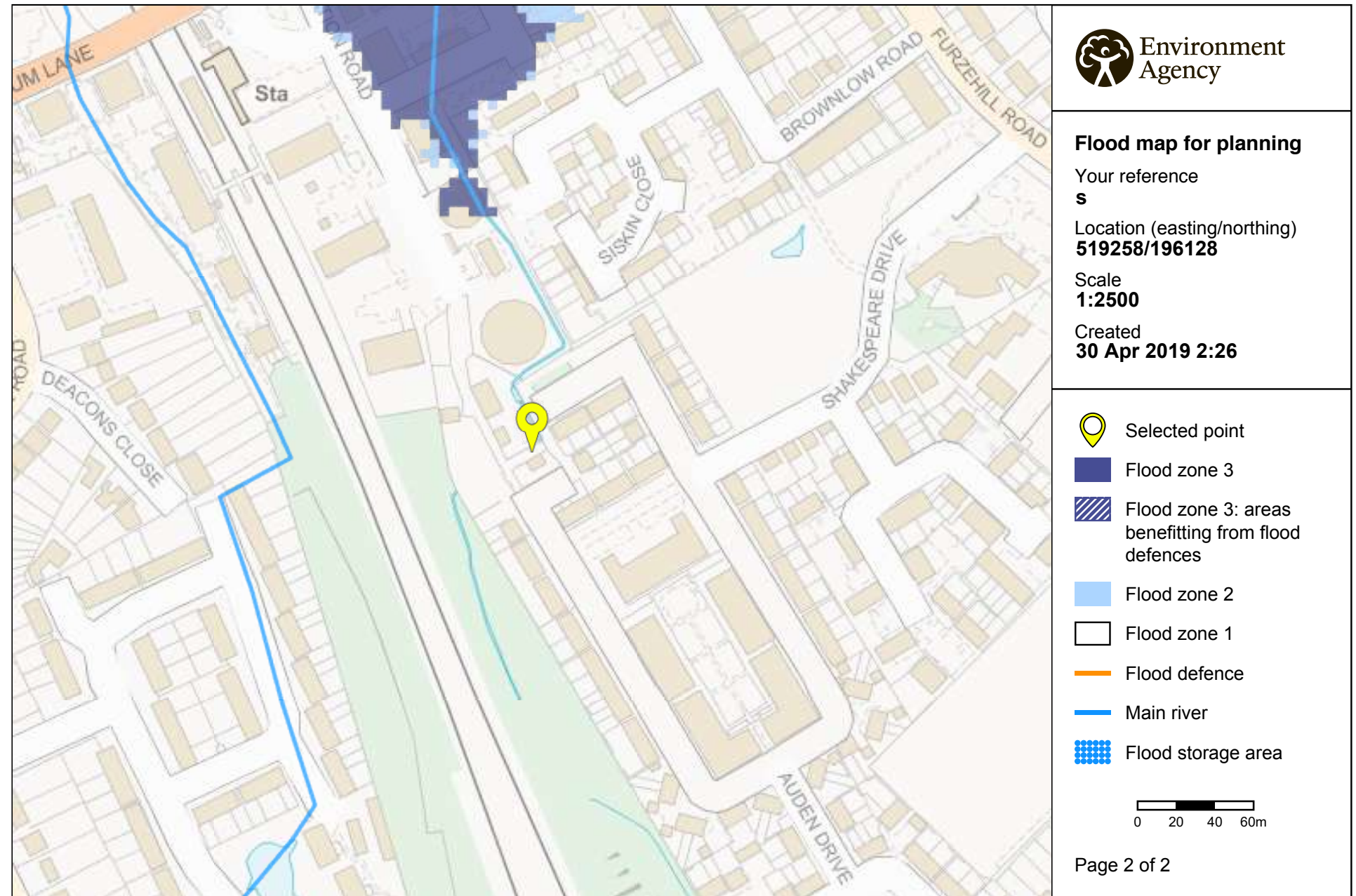
The detailed view shows that the Low Risk Depth is below 300mm. Residential development is considered more vulnerable, therefore most appropriate in Zone 1.

Flood Zones	Flood Risk Vulnerability Classification				
	Essential infrastructure	Highly vulnerable	More vulnerable	Less vulnerable	Water compatible
Zone 1	✓	✓	✓	✓	✓
Zone 2	✓	Exception Test required	✓	✓	✓
Zone 3a †	Exception Test required †	X	Exception Test required	✓	✓
Zone 3b *	Exception Test required *	X	X	X	✓*

Key:

✓ Development is appropriate

X Development should not be permitted.



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3.0 Previous planning history

The submitted proposal builds on the Pre-application advice feedback (18/0193/PA) received on the 29th January 2019.

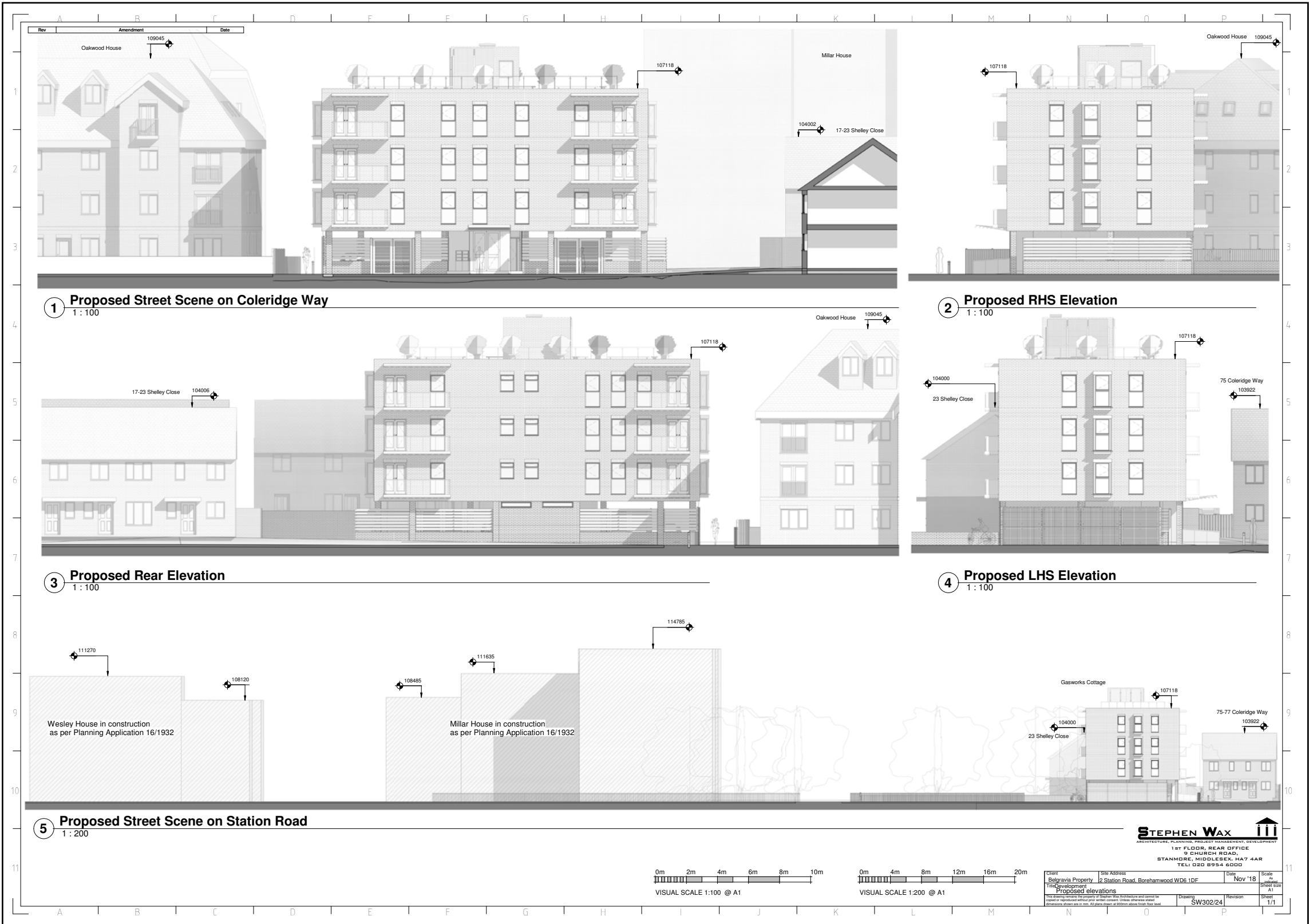
Care has been taken while revising the design of the scheme to respond to the council's comments. Key changes made in response to the comments are:

- Grey brick changed to London Stock brick
- 'Dead' frontage changed to active frontage with relocation of car park and access directly off Station Road.
- Oriel windows changed to louvred windows to protect privacy of neighbouring building.
- Asymmetry brought to the facade
- Inclusion of more family sized 3 bed apartments
- Removal of green walls
- Reduction in the amount of parking
- General reduction of bulk and massing
- Removal of protruding balconies to 'front' elevation.



Plans proposed at pre-app 18/0193/PA

3.0 Previous planning history



Elevations proposed at pre-app 18/0193/PA

4.0 Proposal

Proposed accommodation

The proposal is for 9 new apartments made up of 2 duplexes (3-bed) accessed directly off Station Road and 7 apartments (1 x 3-bed, 6 x 2-bed 1 x 1-bed) accessed off a communal entrance stair and lift core off Coleridge Way.

The ground floor is recessed to the South to allow for 6 parking spaces off Coleridge Way. All apartments have either a garden, balcony or roof terrace. Overlooking issues of neighbouring properties to the East are eliminated with louvres. The apartments comply with the 'Technical housing standards – nationally described space standard'

The area schedule below shows the proposed apartments against the Technical housing standards.

Apartment Number		GIA per unit (SQM)		Built-in Storage (SQM)		Outdoor ammenity (SQM)
		Proposal	minimum per technical housing standard	Proposal	minimum per technical housing standard	
Ground floor						
1	3b 4p duplex	97	84	4.7	2.5	19
2	3b 4p duplex	86	84	3.1	2.5	7.5
3	2b 4p flat	77	70	4.6	2.0	28.5
1st floor						
4	3b 5p flat	86	86	3.6	2.5	11.8
2nd floor						
5	2b 3p flat	71	61	2.2	2.0	8.3
6	2b 3p flat	71	61	5.4	2.0	8.3
7	1b 2p flat	50	50	2.2	1.5	5
3rd floor						
8	2b 4p flat	81	70	5.4	2.0	18.8
9	2b 4p flat	75	70	3.8	2.0	11.5
Total		693.2				



4.0 Proposal



View from Coleridge Way looking North



View of Station Road looking North



Front doors to Duplexes on Station Road



Front door to stair and lift core on Coleridge Way



Front door to stair and lift core on Coleridge Way



View from Station Road looking South



View from Shellev Close



View from Coleridge Way looking West



View from Station Road looking South

4.0 Proposal

Materials

The material palette has been chosen to both respond to the existing and proposed developments in the area whilst also defining its own character. The palette is a simple composition of brick, concrete and metal with a focus on quality materials.



1
Expressed concrete slabs

Horizontal bands of lightly coloured concrete running around the building break up the elevation and frame the building. Full height recesses for windows and balconies span from band to band.



2
London Stock brickwork

In keeping with the street scene of Coleridge Way a yellow brick is proposed for the principal material. London Stock brick in two tones, lighter for the upper floors and darker for the ground floor.



3
Bronze metalwork

Bronze colour aluminium balustrading, windows and doorways compliment the concrete and brickwork.



4.0 Proposal

Active frontage to Station Road

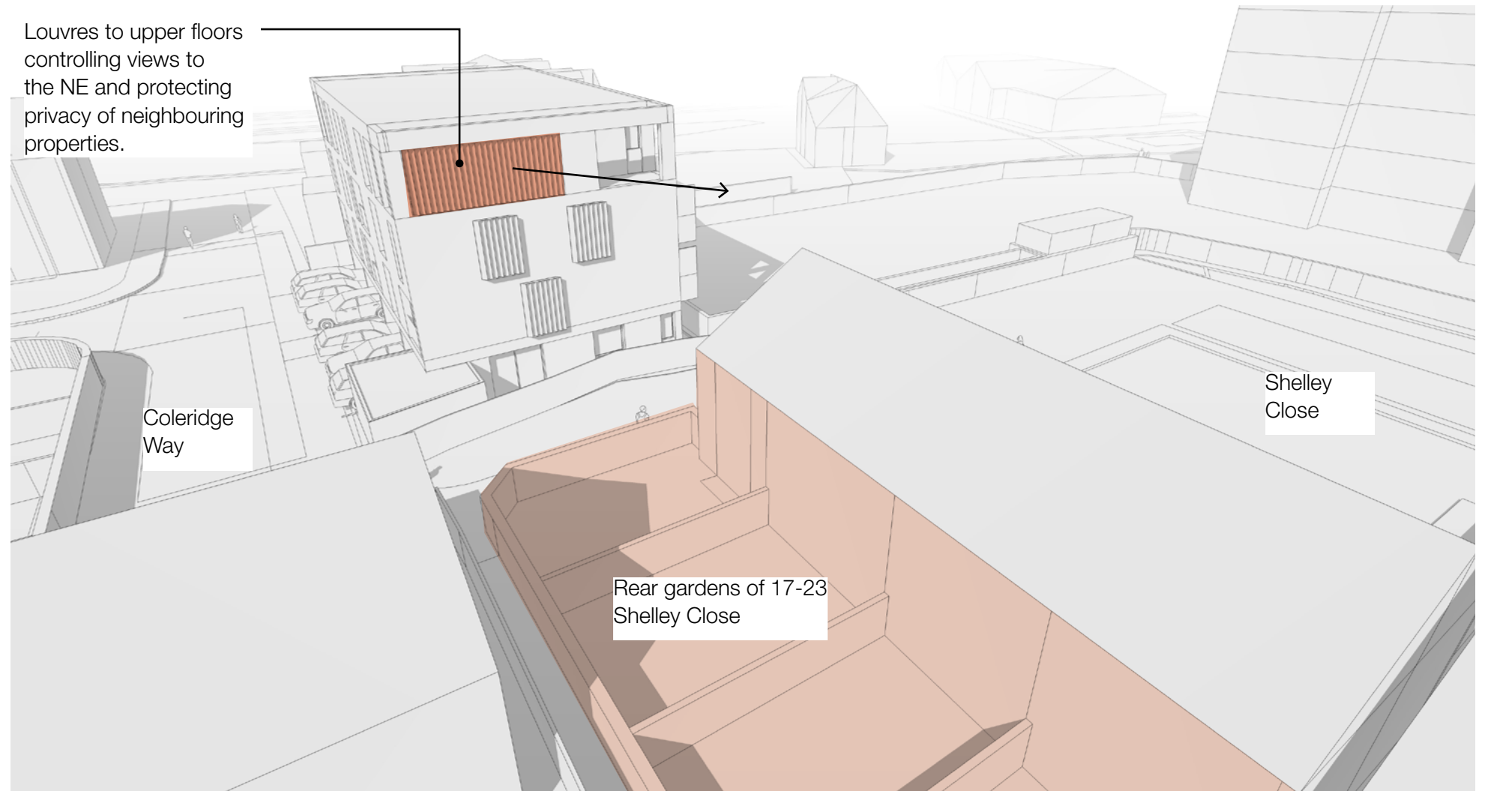
In order to prevent creating a 'dead' frontage onto Station Road to the West it is proposed that the front doors to the duplexes be located here straight off the street. In addition, the living rooms windows have been positioned to overlook Station Road. To improve a sense of security to the duplexes while still providing the overlooking it is proposed that the ground floor windows on Station Road be protected behind a hit an miss brick pattern.



Protecting the privacy of neighbours

The site has the potential to overlook the rear gardens of 17-23 Shelley Close to the East of the site. To prevent this issue louvres are proposed over the windows of 1st and 2nd floor and to the roof terrace of 3rd floor. The roof terrace to 3rd floor is restricted to the SE corner only to reduce the amount of full height louvres required and therefore reducing the bulk of the building.

We have considered the 'front to back' distances between the subject site and the houses that front Shakespeare Drive and confirm that we are 23m away.



4.0 Proposal

Secure by Design

Running along the western boundary of the site is Station Road. It is a narrow, badly lit passage with little overlooking from neighbouring buildings. To improve the security on the street the proposal includes front doors to the duplexes directly off Station Road together with windows overlooking it.

Windows at ground level on Station Road are protected behind a hit and miss brick facade. The canopy over the duplex entrances will be well lit to prevent dark areas at night where people can hide.

The front doors are flush with the building line so as not to provide hiding spaces. Letter boxes mounted in the wall next to the front door communal entrance allow for through door mail delivery and therefore removes the need for workers to enter the building. The semi-undercroft area is open and well lit with good overlooking from the ground floor flats.

A small communal parking area is provided adjacent to users properties. The car park is overlooked from both the street level and above from the apartment windows of the building. As the parking is only for use of the building, users will recognise the car owners. There is no access via carpark to the more vulnerable rear of the dwelling.

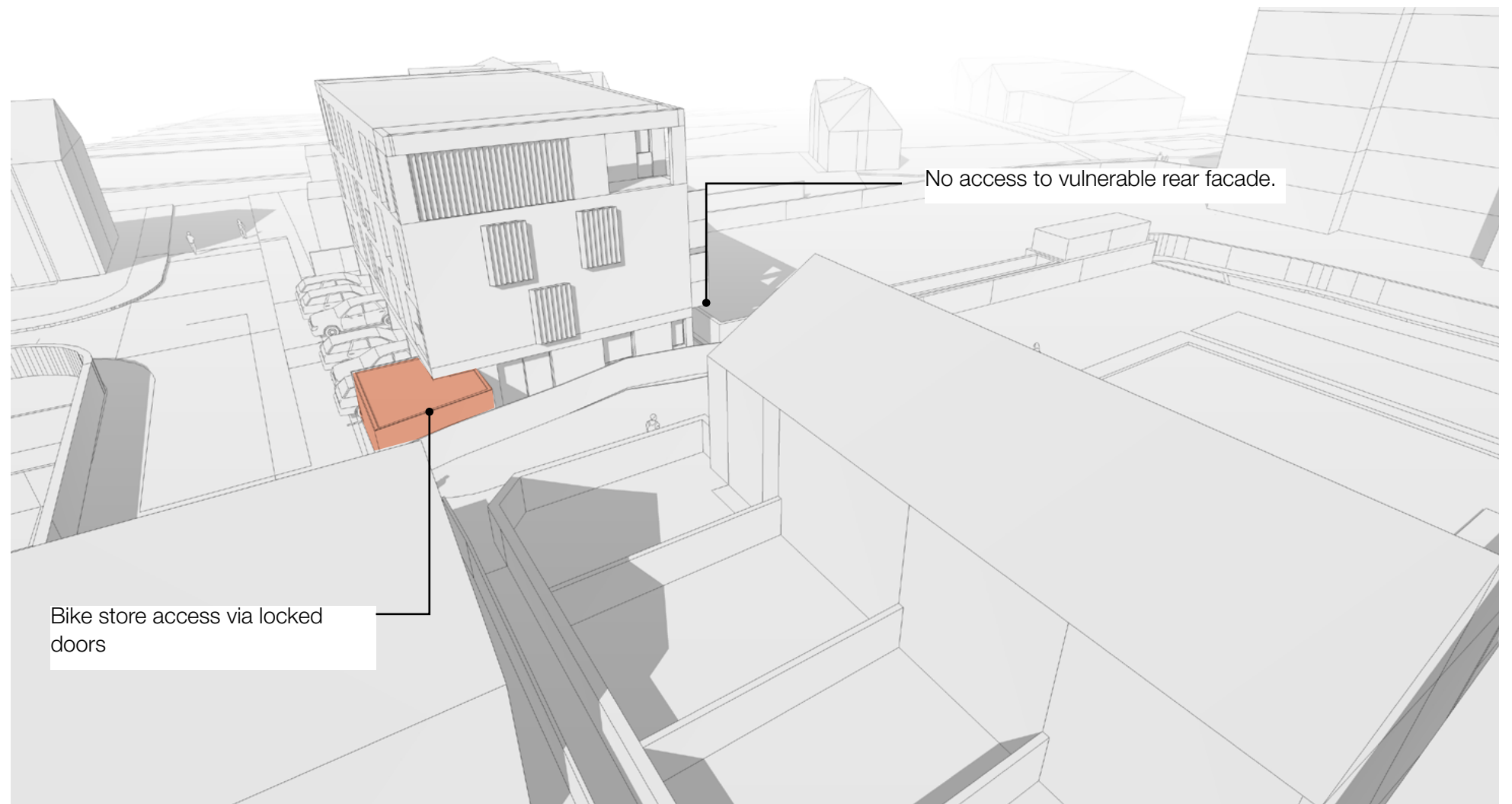
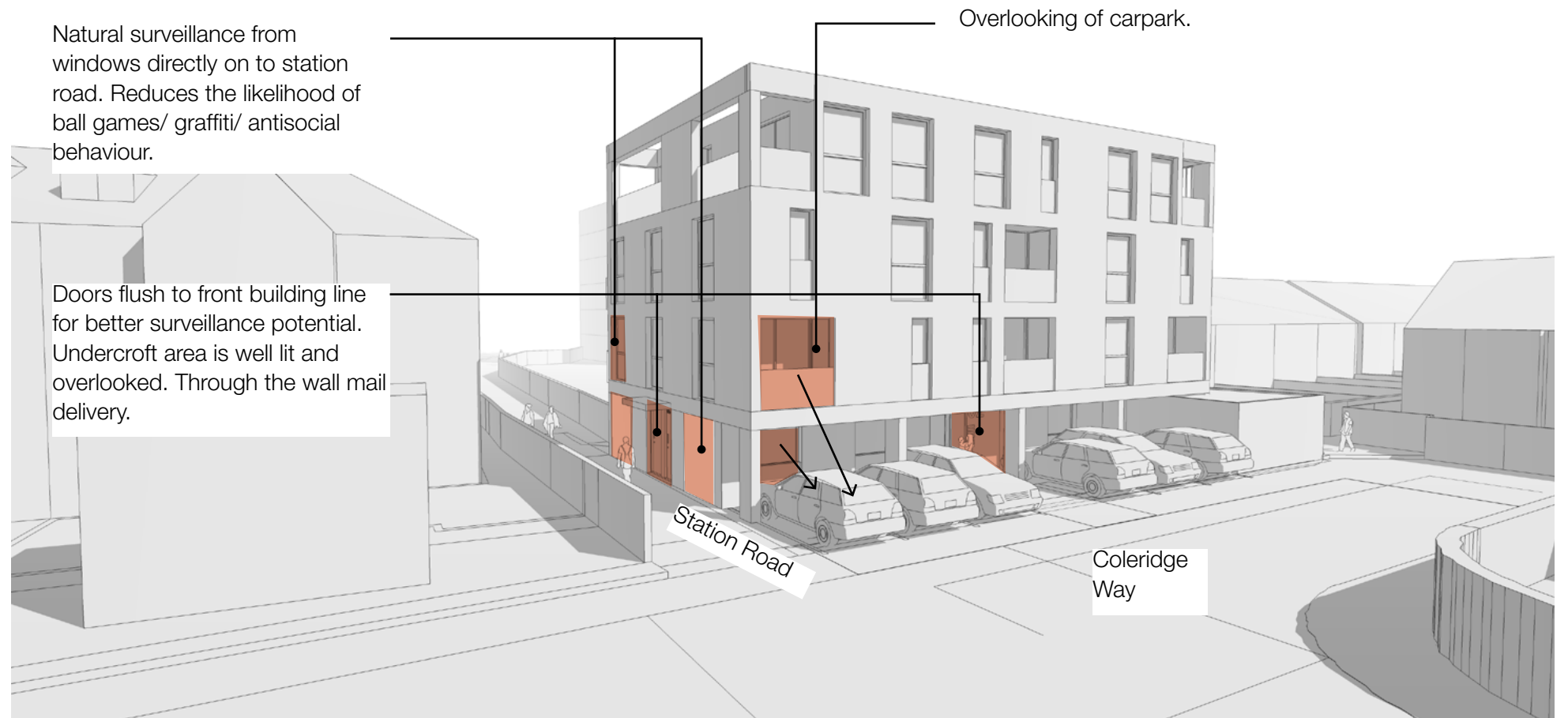
The bike and bin stores are secured behind a locked door.

Sustainability

There is an electrical vehicle charging point beside the bikes and bin store.

Demolition

The proposal involves the demolition of the existing garages, dilapidated cottage and site boundaries. The 4 trees on the site are also to be removed.



4.0 Proposal

Parking provision and impact on Highways

Given that the site is located just a few minutes walk from Elstree and Borehamwood train station, the site is in Residential Accessibility Zone 3 and the surrounding streets are all covered by parking restrictions only 6 parking spaces are proposed for the site. Refer to the Transport statement for more information.

The proposal includes 6 off-street parking spaces directly off Coleridge Way. The ground floor is recessed by 5.3 m along the south side to allow for parking, this provides an additional buffer from the 4.8 m parking standard required for a space. There is no vehicular access via Station Road.

Access

Access to the building from the street is via level pavement to the front doors. Within the communal entrance to the apartments is a stair and lift core providing disabled access to the front doors of each property.

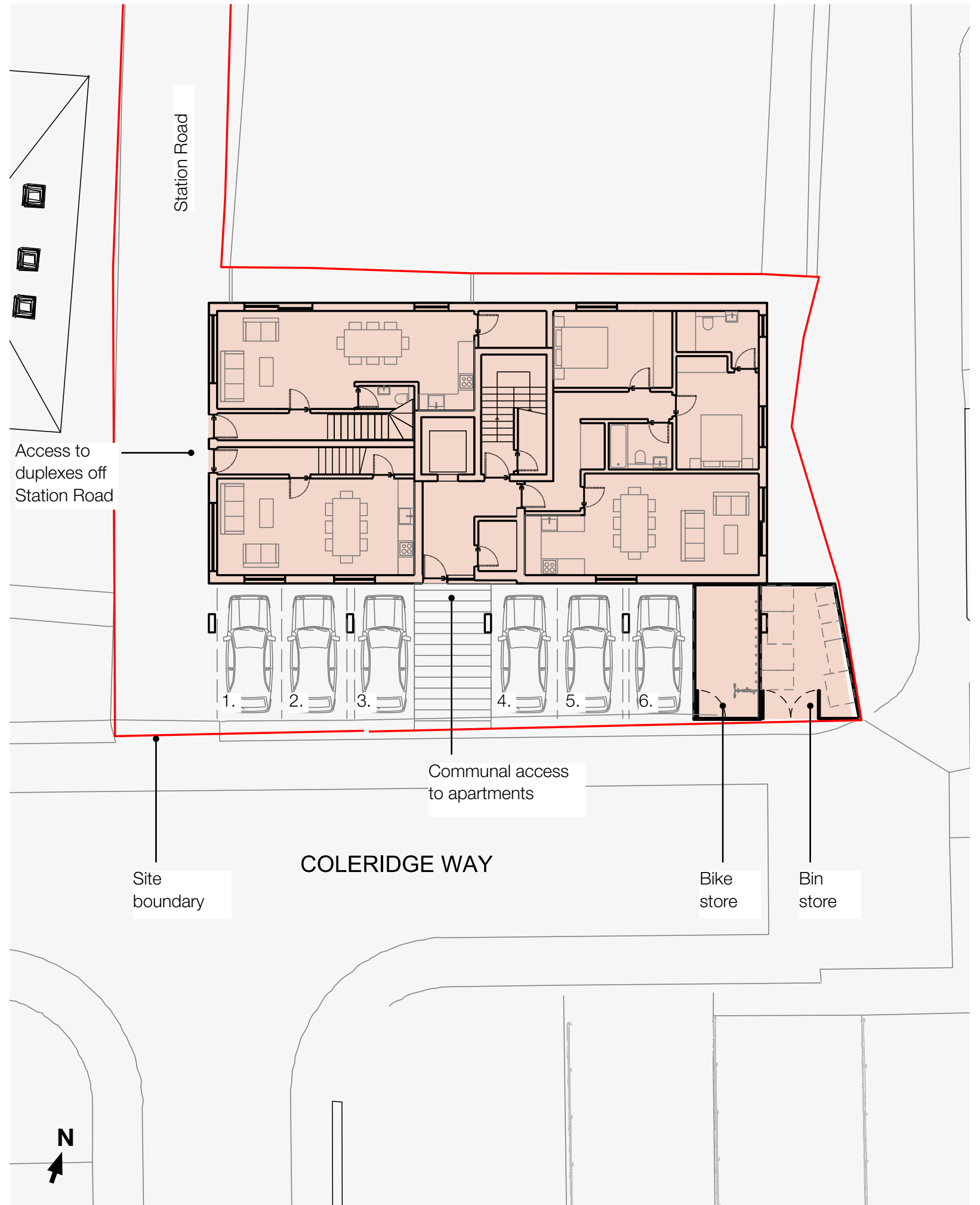
Bike parking

11 bicycle parking spaces have been provided in a secure weatherproof store to the East of the site.

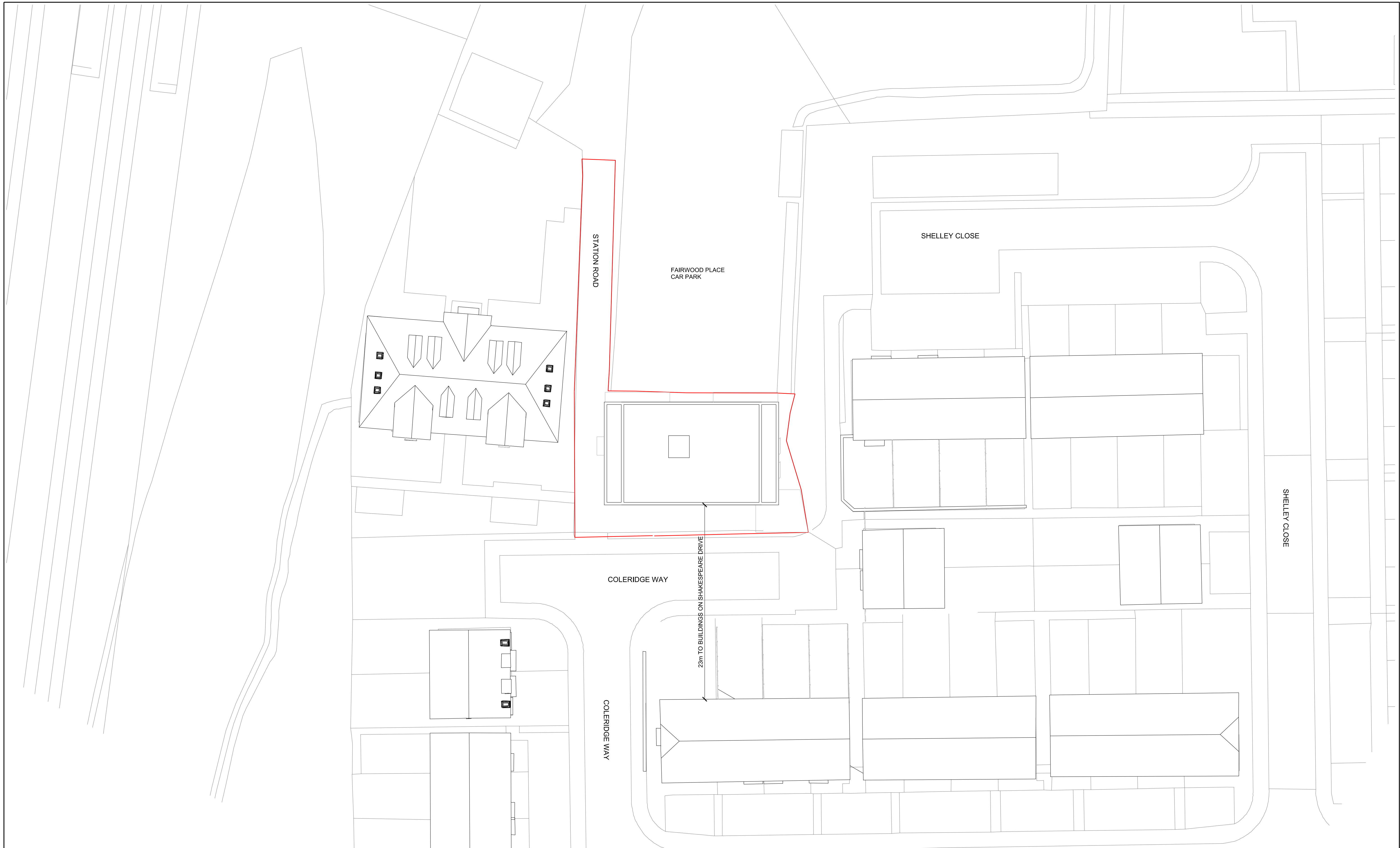
Waste and recycling store

Space has been provided for wheelie bins in a secured bin store adjacent to the bike store. Access is directly off Coleridge Way. The table below shows requirement calculations for the 9 apartments

	Required capacity per flat (L)	Total capacity for 9 flats (L)	Containers to meet required capacity	Total capacity (L)
General waste	240	2160	2 x 1100L	2200
Green waste	80	720	1 x 240L 1 x 660L	900
Commingled recycling	240	2160	2 x 1100L	2200
Paper recycling	38	342	2 x 240L	480
Possible future waste and storage provision	55	495	1 x 660L	660
Total		5877	3 x 240L 2 x 660L 4 x 1100L	6440

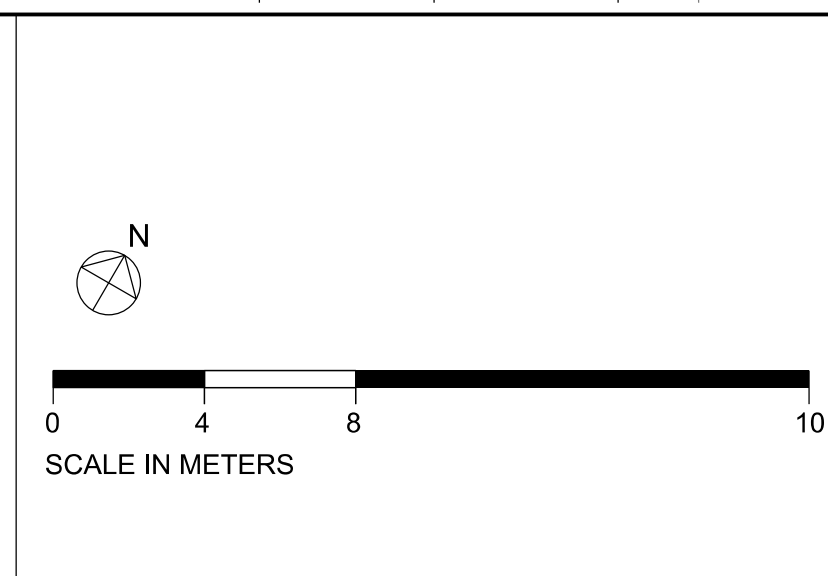


Appendix



KEY

REV	DATE	DESCRIPTION
P1	21/05/2019	PLANNING
P2	02/07/2019	PLANNING UPDATE



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CONTRACTOR:
STRUCTURAL ENGINEER:
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COST CONSULTANT:
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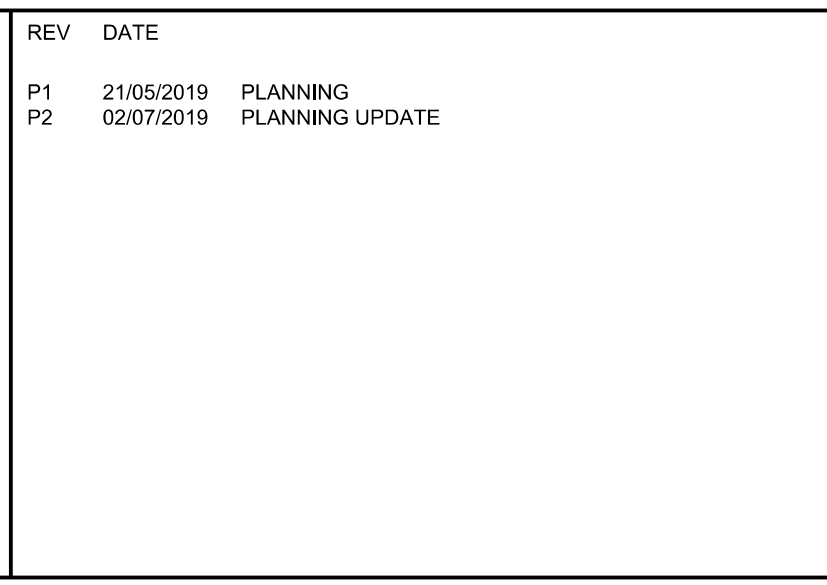
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REV	DATE	DESCRIPTION
P1	21/05/2019	PLANNING
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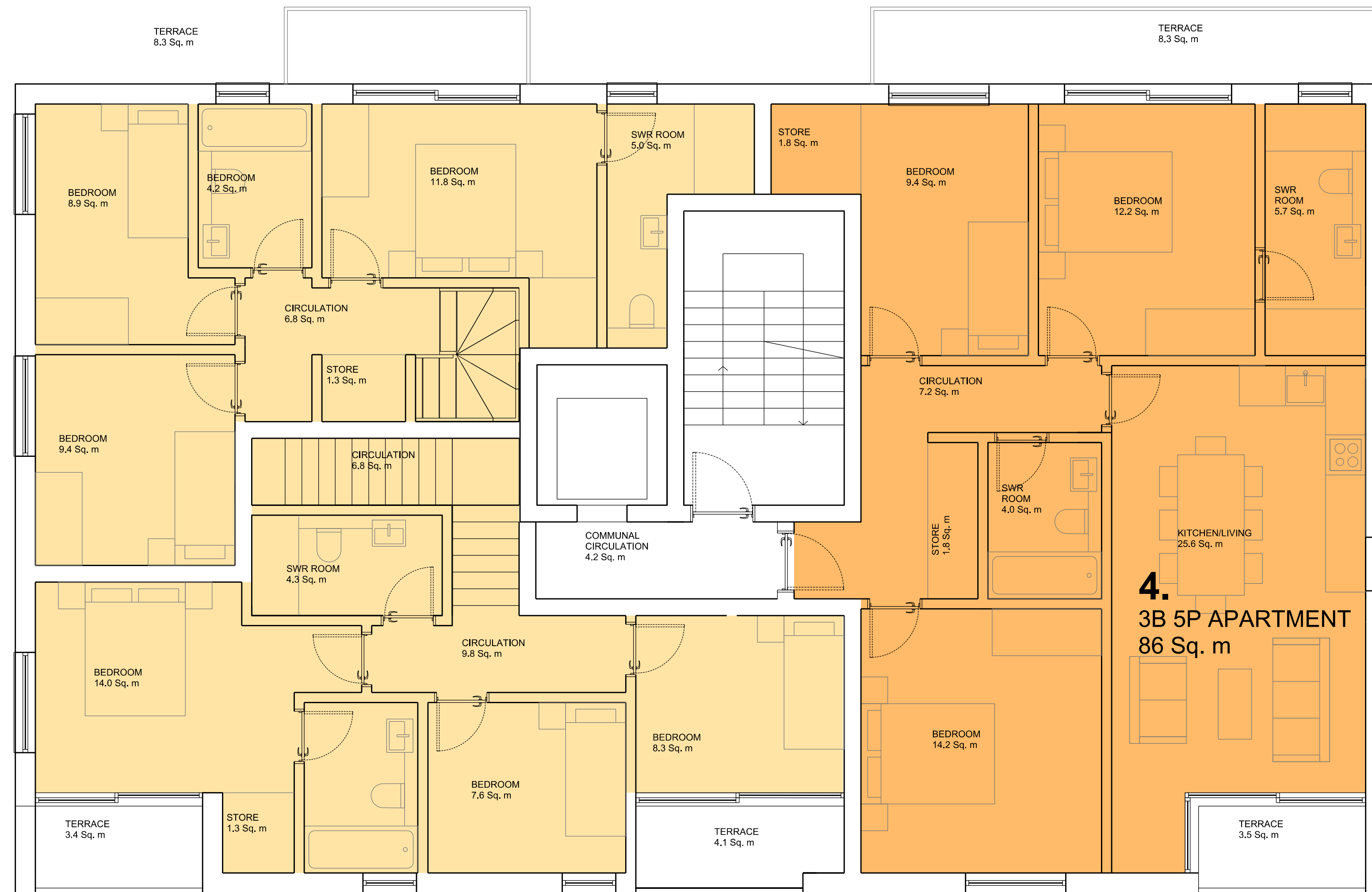
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LOCATION
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GROUND FLOOR PLAN

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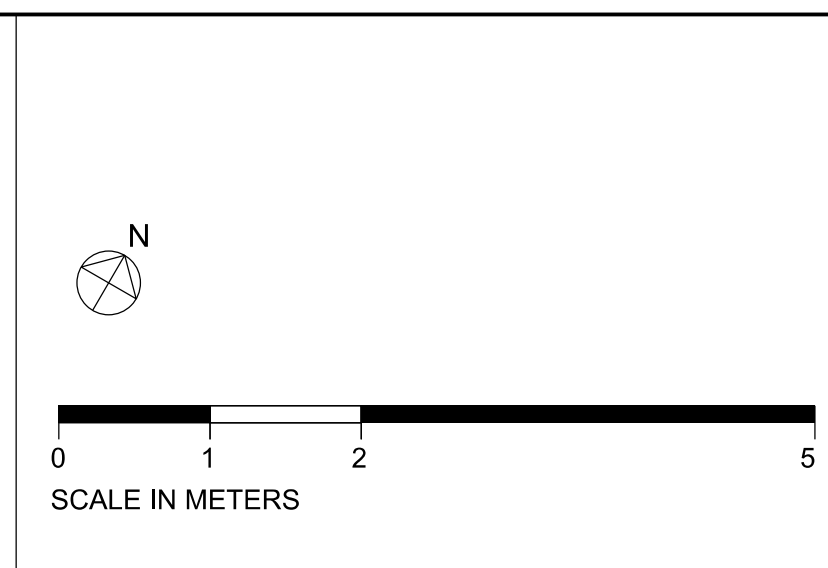
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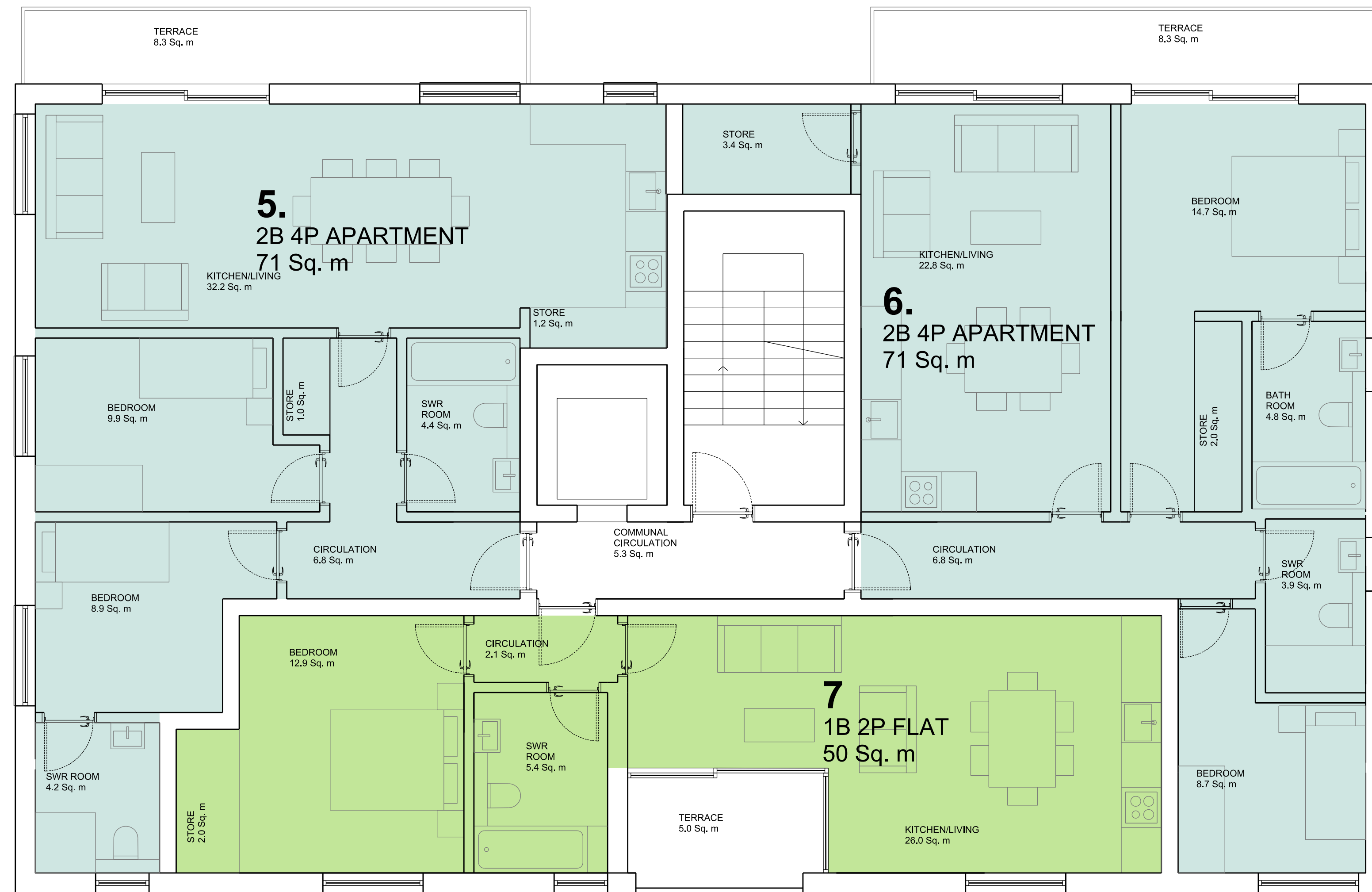
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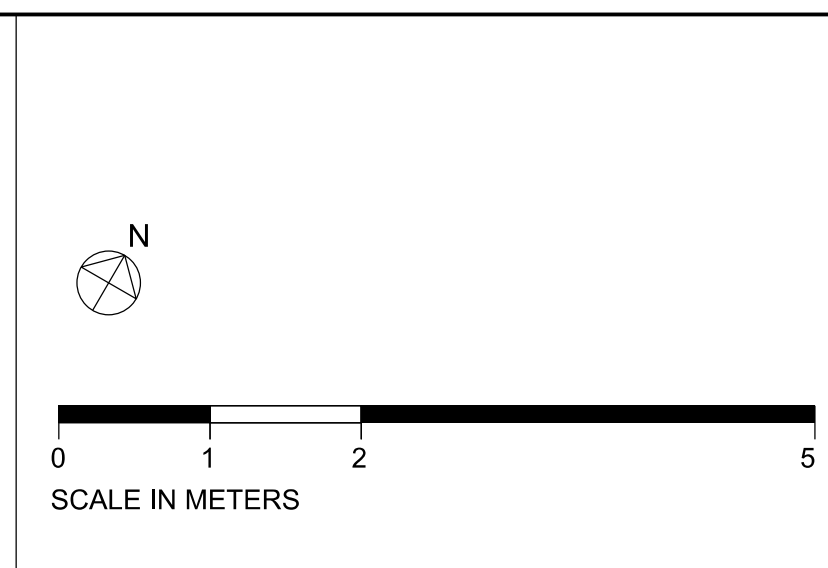
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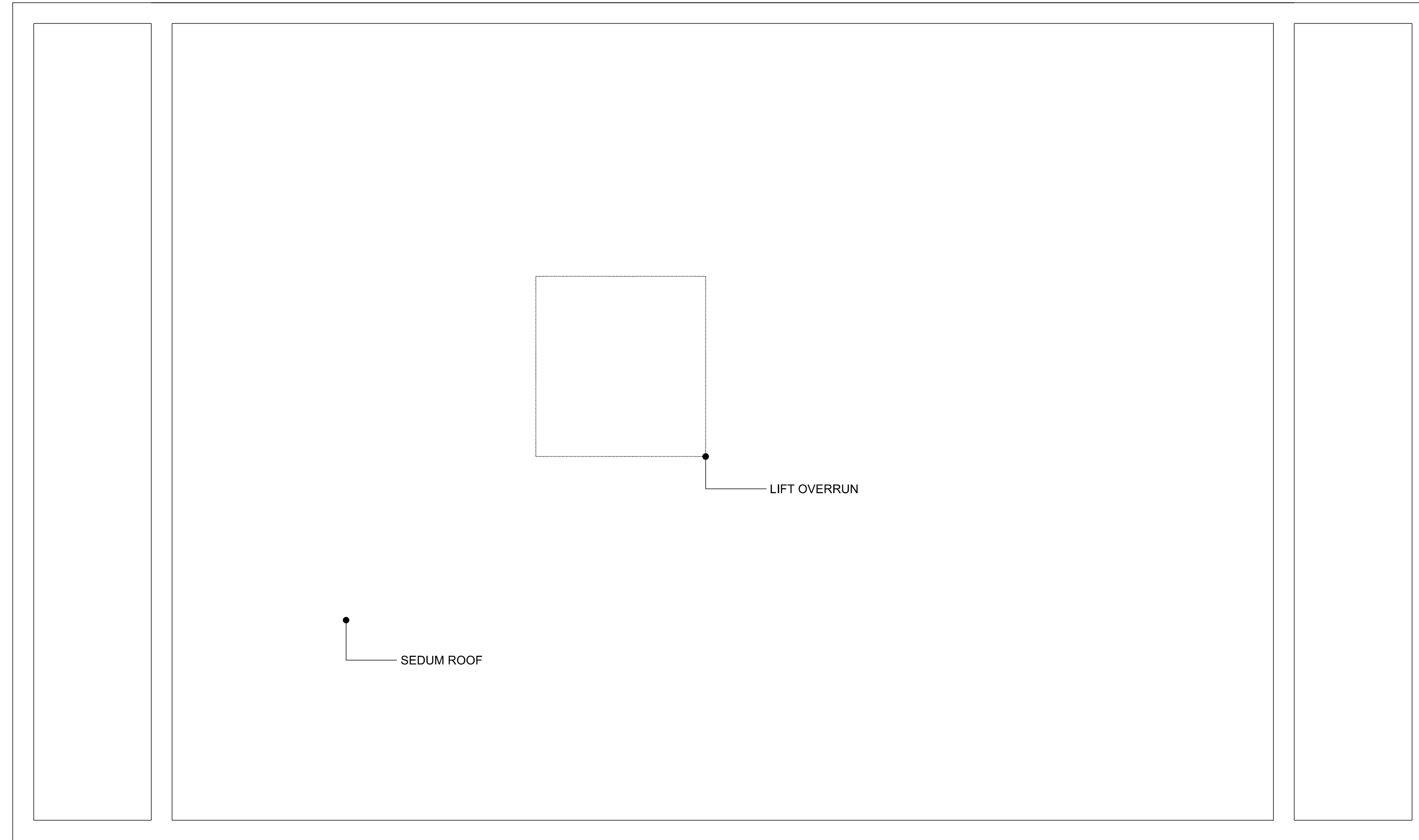
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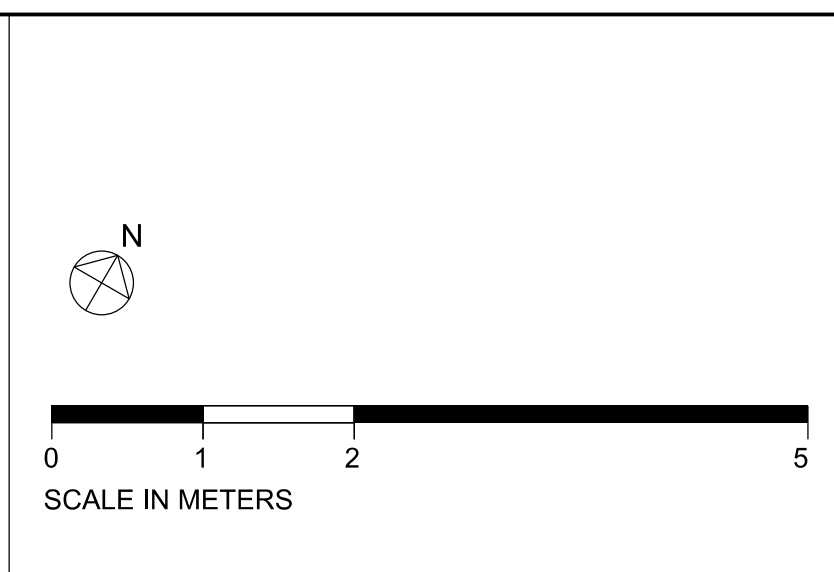
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drawing title / location: 2ND FLOOR PLAN



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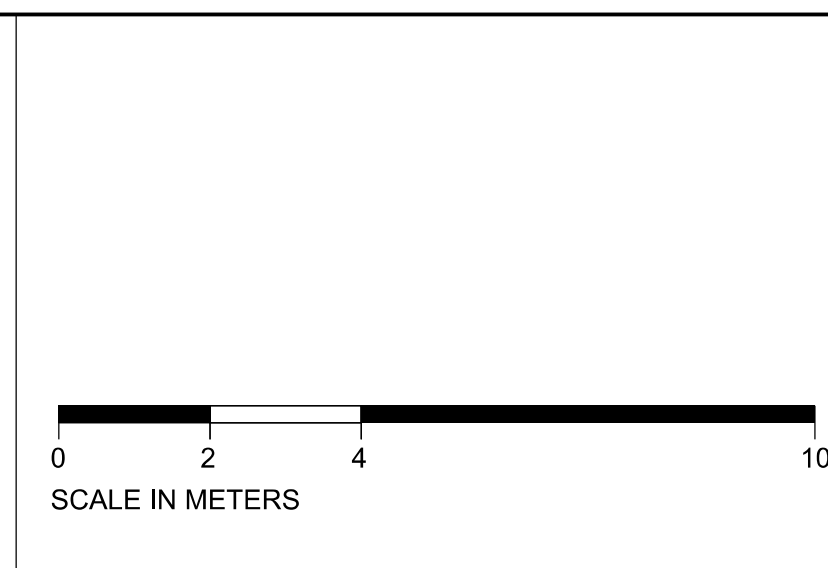
SOUTH ELEVATION



NORTH ELEVATION

KEY

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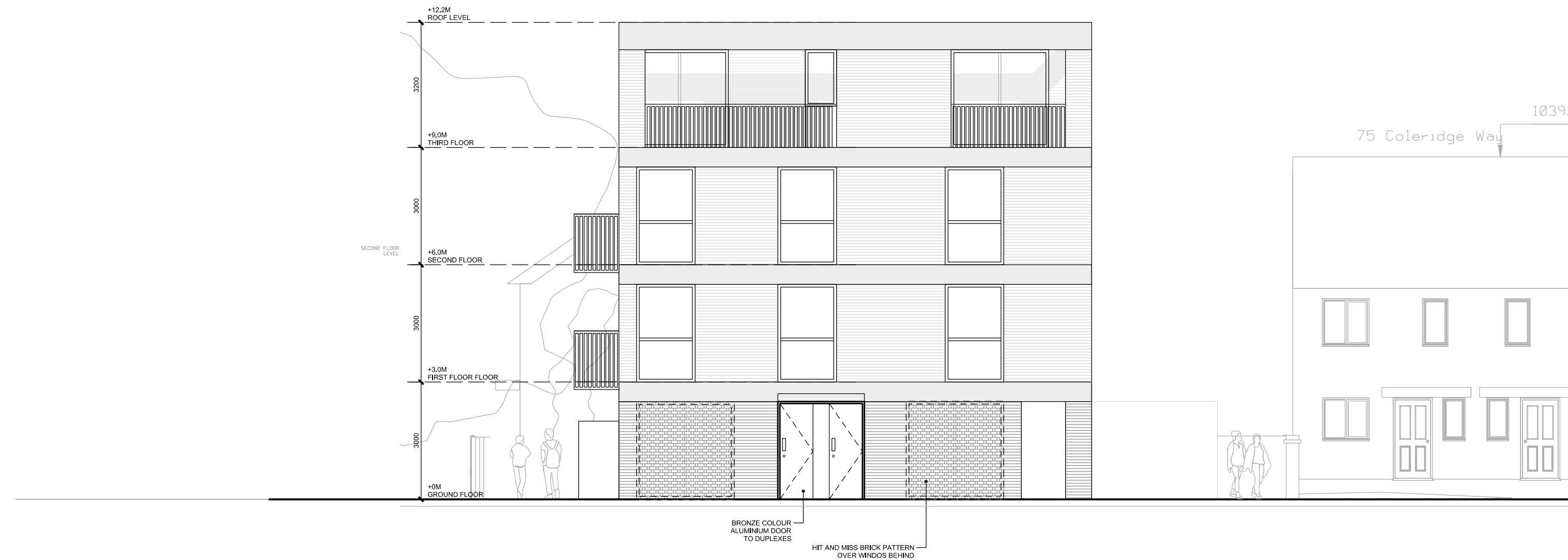
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PROPOSED ELEVATIONS

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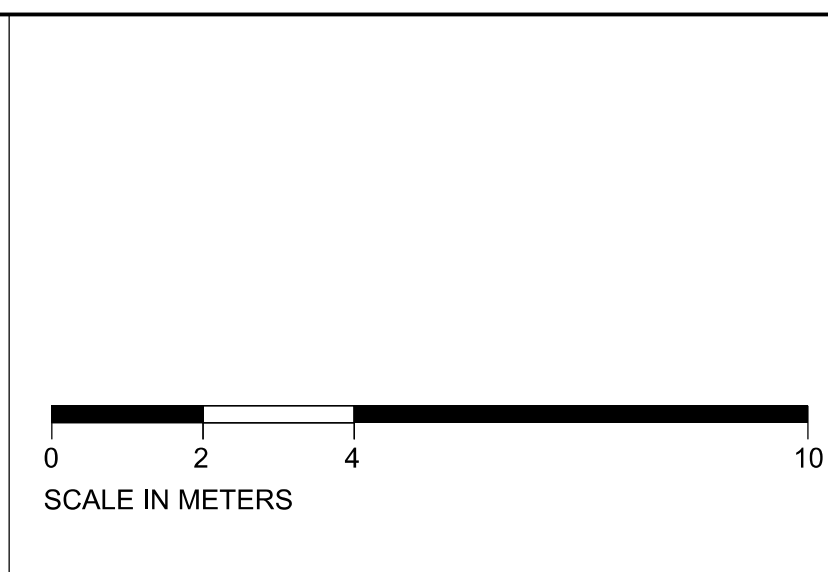


WEST ELEVATION



EAST ELEVATION

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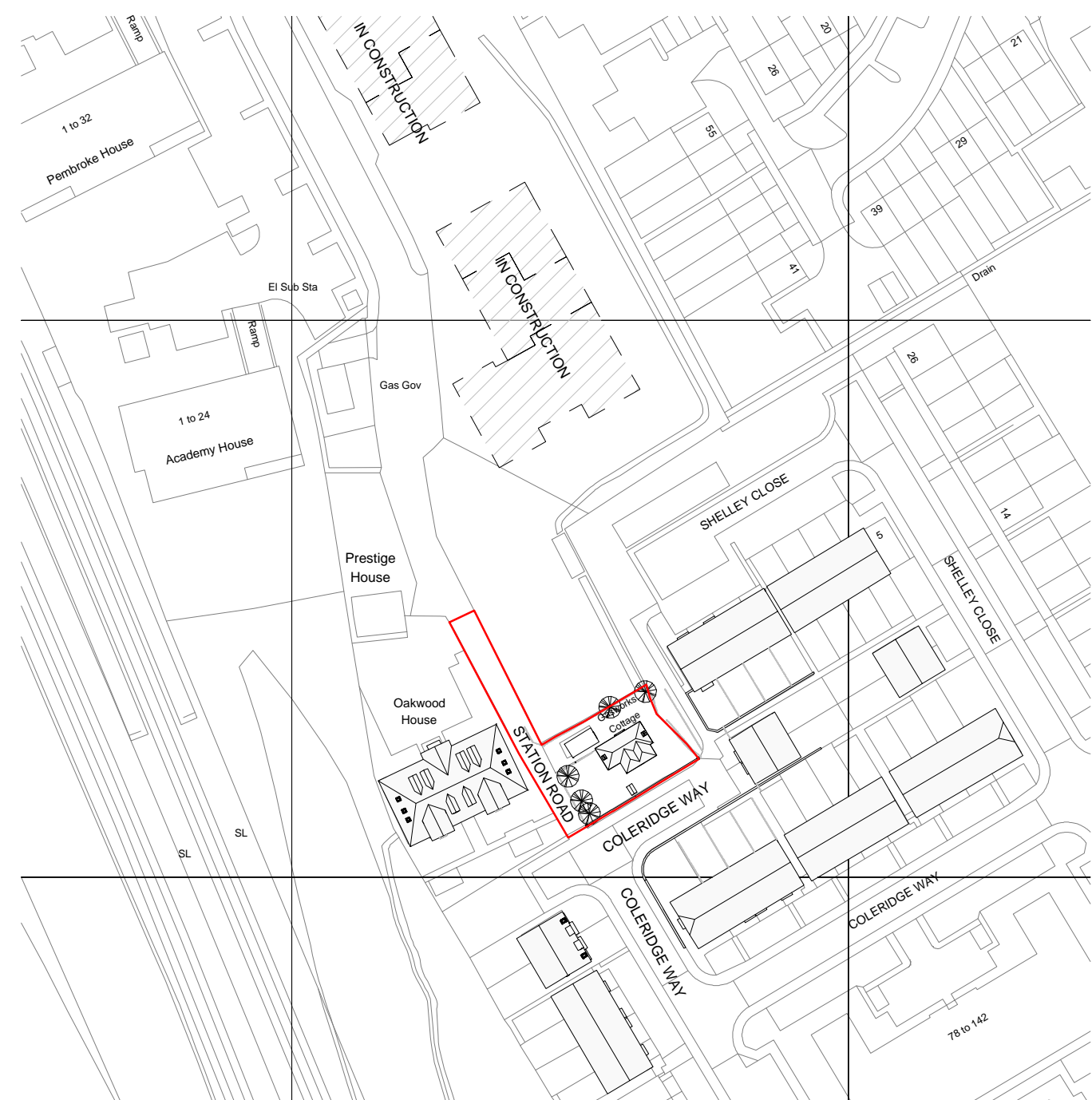
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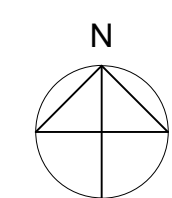
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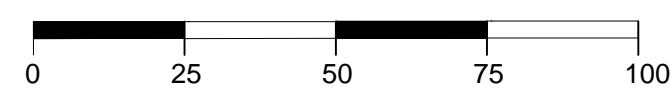
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revision	P2



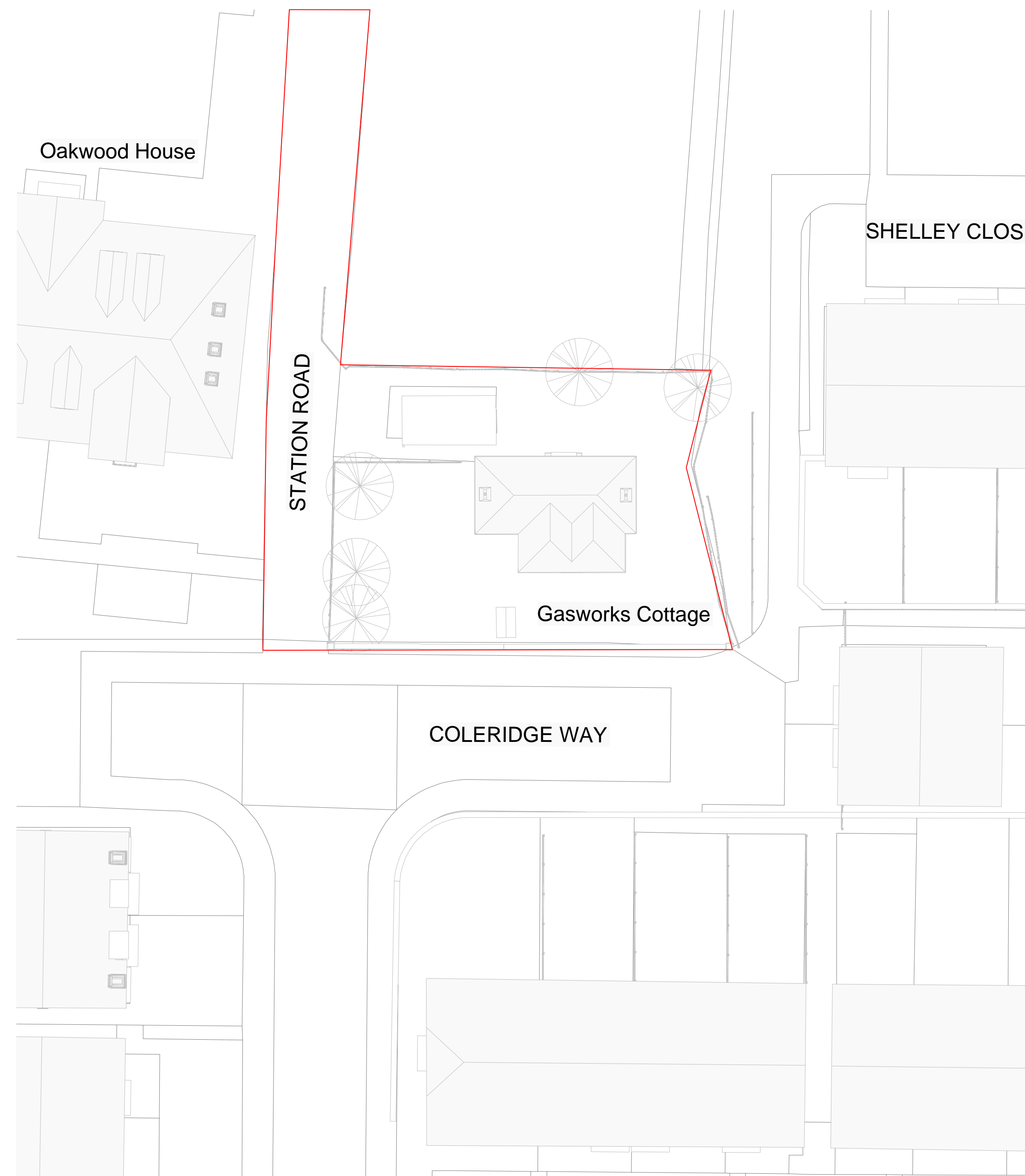
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SCALE : 1:1250



SCALE IN METERS



EXISTING BLOCK PLAN



SCALE : 1:200



SCALE IN METERS

KEY

REV	DATE

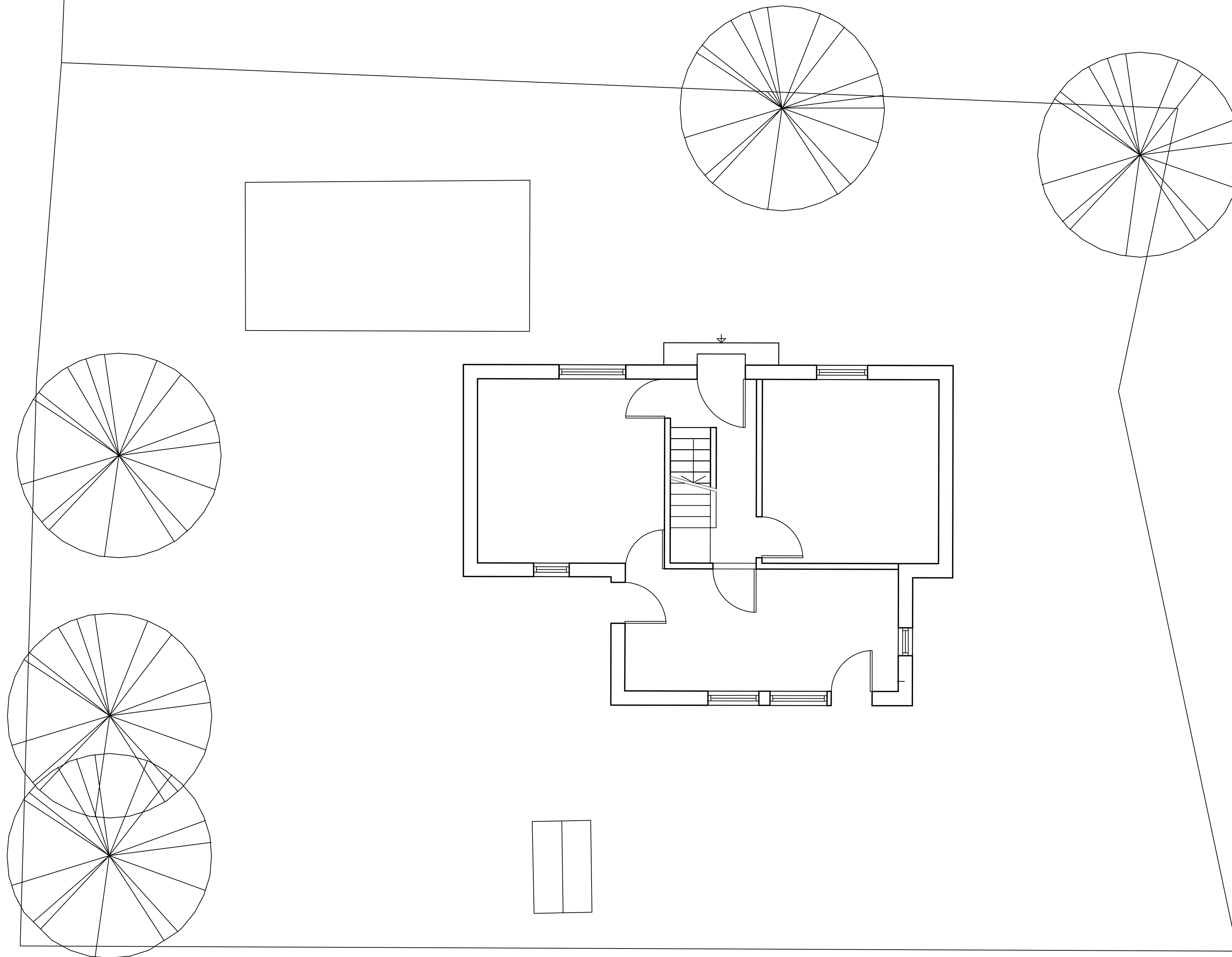
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CLIENT: -
CONTRACTOR: -
STRUCTURAL ENGINEER: -
MECHANICAL ENGINEER: -
COST CONSULTANT: -
PROJECT MANAGER: -
ACOUSTIC CONSULTANT: -
CLADDING CONSULTANT: -
PLANNING CONSULTANT: -

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LOCATION

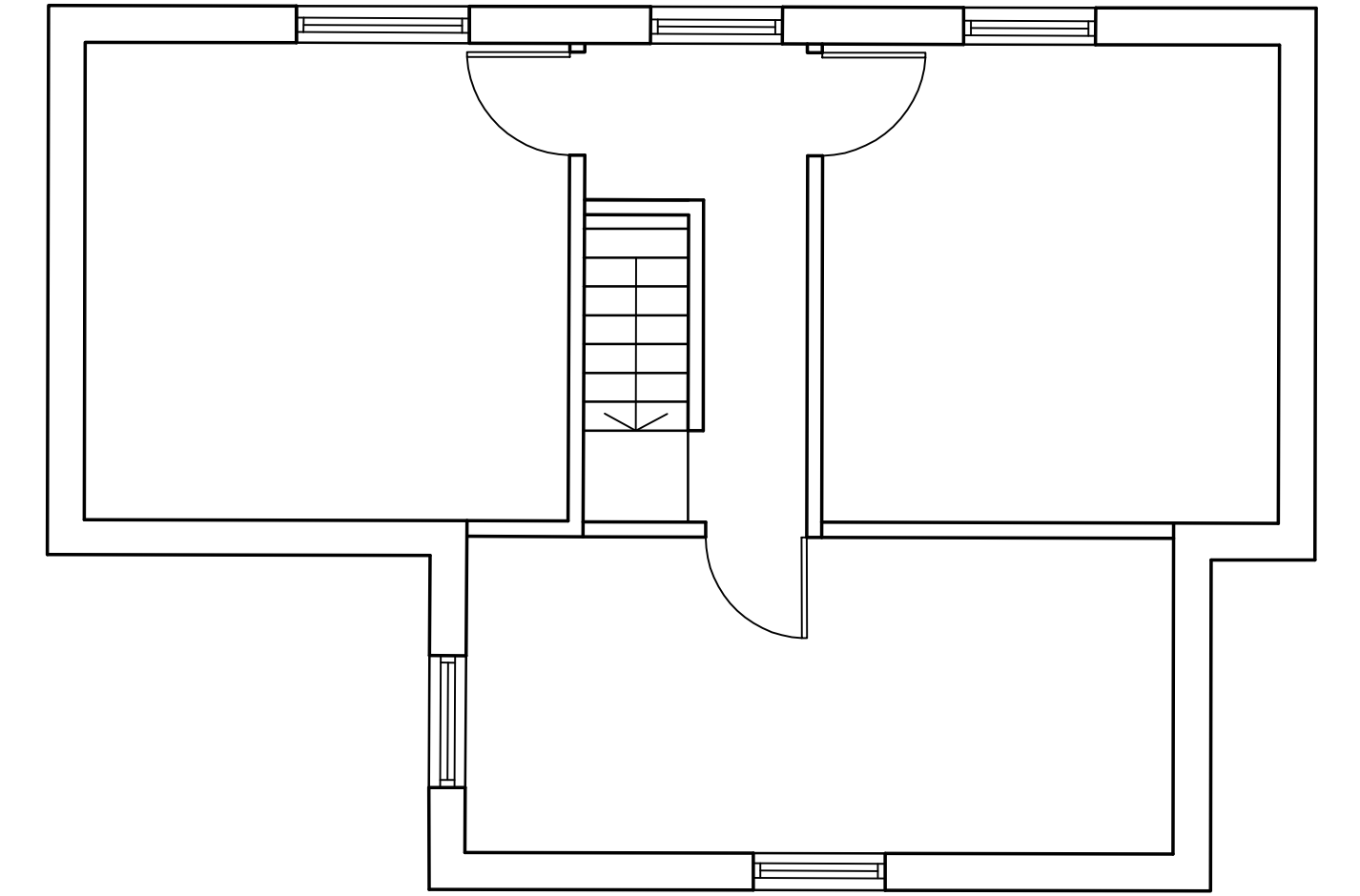
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project	zone	type	classification	drawing no	revision
5286			01	001	

STATION ROAD

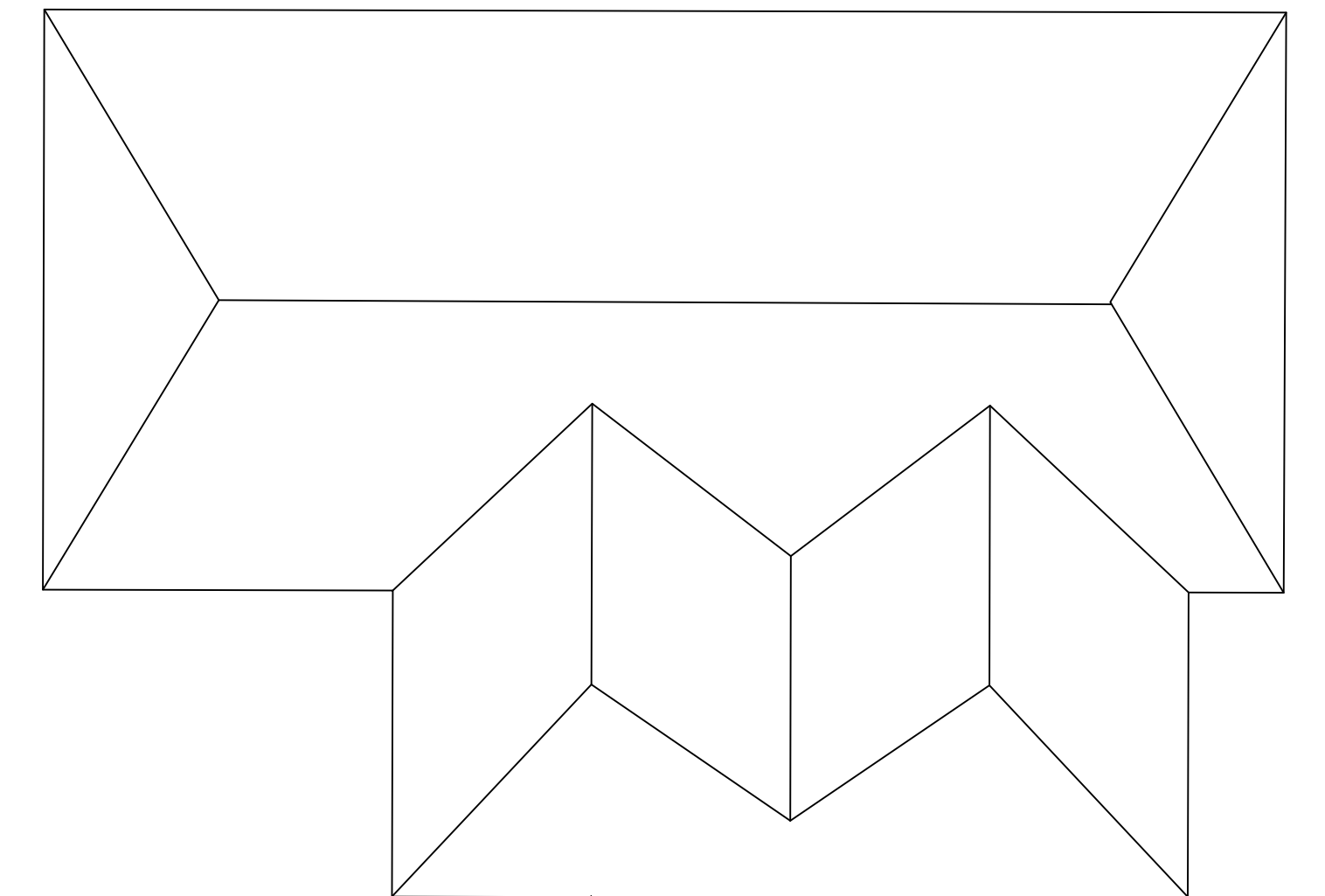


COLERIDGE WAY

EXISTING GROUND FLOOR PLAN



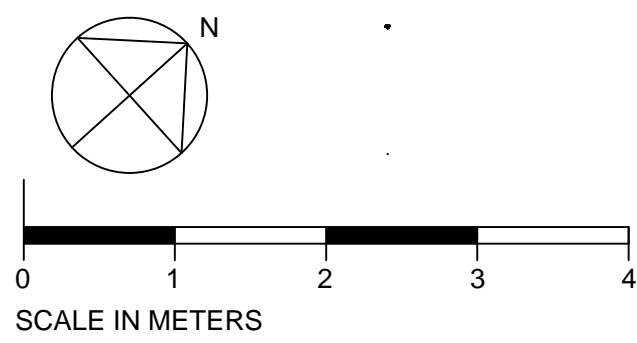
EXISTING FIRST FLOOR PLAN



EXISTING ROOF PLAN

KEY

REV DATE



CONSULTANTS

CLIENT:	
CONTRACTOR:	-
STRUCTURAL ENGINEER:	-
MECHANICAL ENGINEER:	-
COST CONSULTANT:	-
PROJECT MANAGER:	-
ACOUSTIC CONSULTANT:	-
CLADDING CONSULTANT:	-
PLANNING CONSULTANT:	-

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LOCATION



job title					
STATION ROAD					
drawing title / location					
EXISTING FLOOR PLANS					
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project	zone	type	classification	drawing no	revision
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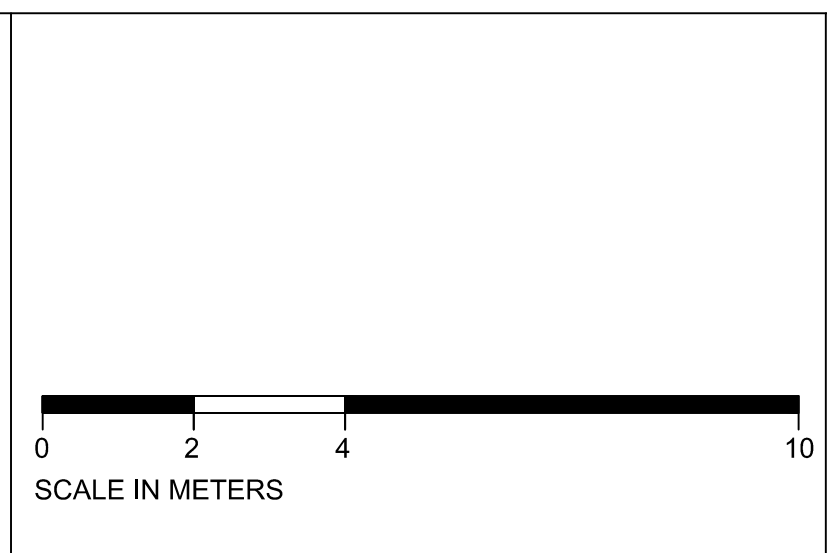
EXISTING SOUTH ELEVATION



EXISTING EAST ELEVATION

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CONTRACTOR: -
STRUCTURAL ENGINEER: -
MECHANICAL ENGINEER: -
COST CONSULTANT: -
PROJECT MANAGER: -
ACOUSTIC CONSULTANT: -
CLADDING CONSULTANT: -
PLANNING CONSULTANT: -



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LOCATION

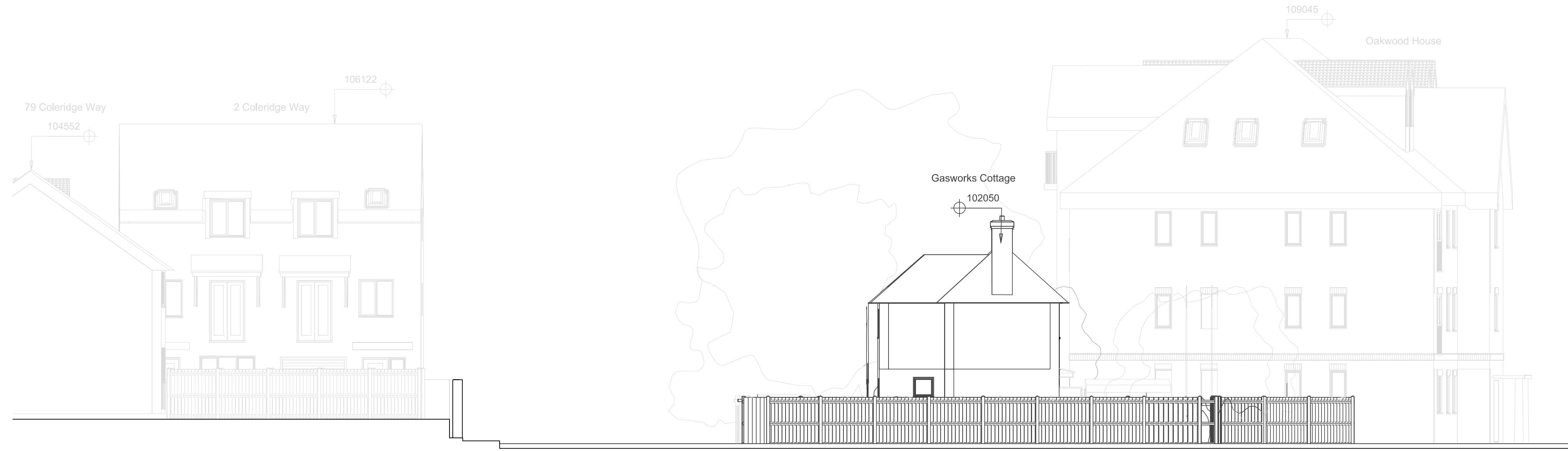
WHITE RED ARCHITECTS
 29 Charlotte Road
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 EC2A 3PB
 TEL: 020 7859 4521
 www.whitered.co.uk

job title
STATION ROAD
 drawing title / location

EXISTING ELEVATIONS

drawn by	checked	scale	status
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project	zone	type	classification	drawing no	revision
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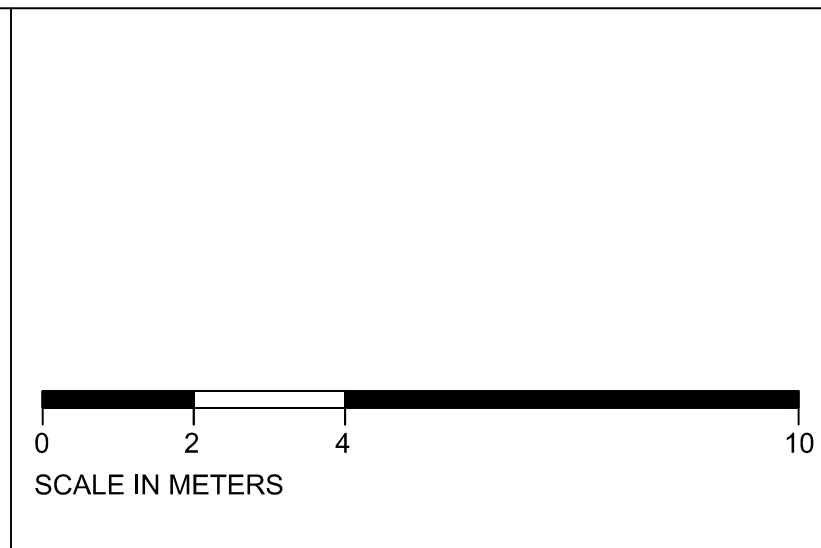
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EXISTING WEST ELEVATION

KEY	REV	DATE

REV	DATE



CONSULTANTS
CLIENT:
CONTRACTOR:
STRUCTURAL ENGINEER:
MECHANICAL ENGINEER:
COST CONSULTANT:
PROJECT MANAGER:
ACOUSTIC CONSULTANT:
CLADDING CONSULTANT:
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LOCATION
STATION ROAD
EXISTING ELEVATIONS
drawn by checked scale 1:100@A1 1:200@A3 status PLANNING
project 5286 zone type classification 01 drawing no 201 revision P1

WHITE RED ARCHITECTS
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job title
STATION ROAD
 drawing title / location
EXISTING ELEVATIONS

drawn by	checked	scale	1:100@A1 1:200@A3	status	PLANNING
project	5286	zone	type	classification	01 201 P1



Planning and Economic Development

Mr Stephen Wax
Stephen Wax Associates
1st Floor, Rear Office
9 Church Road
Stanmore, HA7 4AR

Our Ref: 18/0193/PA
Contact: Max Sanders
Extension: 0208 207 22 77 Ext 5187
Email: max.sanders@hertsmere.gov.uk
Date: 29th January 2019

Dear Mr Wax

2 Station Road, Borehamwood, WD6 1DF Pre-application planning advice 18/0193/PA

I am writing to set out the advice that I gave you verbally when I met with you and your architect Jonathan Ellis on site on 17th January. Thank you for sending me the minutes of that meeting last week – I have already responded with some comments and clarifications.

Your proposal is to demolish the existing two storey house and to erect a four storey block of nine apartments, with a flat roof. The ground floor would contain an entrance lobby with a lift and stairwell, and with storage lockers, but mainly it would consist of undercroft parking for six cars, with another five parking spaces on the forecourt. There would be a communal front entrance facing south down Coleridge Way.

In my opinion the key issues in this case are as follows:

Key issues

- Demolition of the existing building
- The principle of the proposed use of the land for a flatted residential development.
- The question of which direction the development ought to face.
- Design, including scale and bulk, architectural style and finishing materials.
- The relationship between the development and the street-scene, and with the experience of pedestrians using the footpaths on either side of the site.
- Parking, transport and highways issues.
- Protecting the amenity of the neighbouring residential premises – particularly as regards their privacy.
- The quality of the accommodation that is proposed.

Demolition of the existing building



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- The quality of the accommodation that is proposed.

Demolition of the existing building

The existing vacant house that stands on the site is neither nationally nor locally listed, and it is not located in a Conservation Area. It is of no particular architectural or historic interest, and it appears anomalous in the street-scene. There is no reason to object to its removal.

The proposed use

The proposals map (sub-map L) of the Hertsmere Local Plan (Site Allocations and Development Management Policies Plan 2016) shows this site as lying between two sites that are allocated for major residential developments, which are sites H2 (to the North) and H3 (to the south of your site). Had your site been included in the housing allocation it would have joined those up to form a single allocation site, but I have not been able to discover any particular reason why that was not done. The most likely explanation is that it was not considered likely at that time that your site would come forward for development within the plan-making period – which is a necessary criterion for the designation of a site in a Local Plan.

The site is a vacant house, and it is located on a residential street (Coleridge Way). As seen from the end of Coleridge Way, the neighbours on the right side and opposite are houses, and the neighbours to the left at Oakwood House are flats. New blocks of flats are under construction to the rear of the site, on land that was previously occupied by gas works. Clearly, given its context, the proposal for a residential development is acceptable in principle on this site.

The Council seek a mixture of housing types (albeit Policy CS7 would not apply to a development of only 9 units); and a location such as this, being close to a station and to the Town Centre, is well suited to flats. There are plenty of houses on Coleridge Avenue and on other streets nearby to the East, so the loss of one house on this site is not considered problematic. It would be outweighed by the benefit of a net gain to the Borough of eight dwellings. The neighbour on the west side of the site is a block of flats (Oakwood House) and others are under construction behind your site (i.e. to the North). The principle of a flattened development is considered acceptable on this site.

Direction

In most cases it is obvious which direction a new development should face, but this is an unusual situation. Your draft design shows the proposed building as facing south so as to front on to the turning head that forms the end of Coleridge Way, and I agree that this is the right decision; but it is worth noting that neighbouring premises are facing in various different directions, which makes for an unusually complicated street-scene.

The neighbours to the right side of your site (east) are terraced houses at 17-23 Shelly Close, and the neighbours to the left side (west) are flats at Oakwood House, and those premises all face north, which is the opposite direction to that which you are proposing, which will result in a “back to front” relationship. Normally one would expect the neighbours on the opposite side of a street to face inwards, but in this case 79-85 Coleridge Way face south, presenting their rear boundaries towards your site. Looking at the existing house that stands on your site, it was not immediately

clear to me which direction it was attempting to face – the impression of this old house being one of architectural indecisiveness.

Notwithstanding the fact that the neighbours face in different directions, I think that you have made the right decision in having your development face South because there is only one street that it can address, which is Coleridge Way, and it is right that a building should front onto the street on which it is located, rather than turning its back or its flank to that street.

A consequence of this confusing situation is that currently the street-scene seems incoherent, with this end of Coleridge Way being little more than a turning head (although it will take on a more important role in the future as it will be the entrance to the Taylor Wimpey development at Housing Allocation site H3). The Council would like to see this end of Coleridge Way given a new sense of purpose and identity (Policy SADM30) and my opinion is that your development would provide a visually satisfying punctuation point marking the end of the street, and addressing those who approach from that street.

An issue that arises from this will be the address – which is not a Planning matter, but which is controlled by the council's Street Naming And Numbering Officer. Currently the site's address is 2 Station Road, although the existing house is not located on that street (it is accessed by a footpath connected to Station Road). I suggest that, once planning permission has been granted, you apply for the creation of a new address on Coleridge Way.

Your development will be unusually exposed because the public will be able to see it from all four directions. It will front onto a public street (Coleridge Way) and there are footpaths down both sides of the site, which are used by local residents to access the station and the Town Centre. The space to the rear will be open as that will be occupied by the car park of the flatted development that is under construction on Housing Allocation site H2 to the North. This will mean that care must be taken to avoid any of the four elevations being blank, featureless or unwelcoming. This is best achieved by including windows in those elevations.

Design

While it is true that the neighbouring buildings on either side of the site, and elsewhere on Coleridge Way and neighbouring residential streets, are characterised by pitched roofs, I never the less consider that your proposal for a flat roof would be acceptable in this location. It will result in a development that appears less tall and overbearing towards its neighbours than it would have done if it had been designed with a flat roof. It would also relate architecturally to the flat-roofed blocks that are currently under construction at Housing Allocation site H2 to the rear. A four storey building with a flat roof would not appear out of keeping in this location, given the scale of the new buildings that are being constructed to the rear, and given the 3-4 storey block of flats on the adjacent site at Oakwood House.

As I mentioned to you when we met on site, I have noticed that the attempt at symmetry for the front elevation has not been entirely successful as regards the fenestration on the upper floors, and I think that this needs some more work – either

through a design that does not attempt symmetry at all, or else by moving the windows of the first, second and third floors, and arranging them symmetrically with reference to the entrance doors as the building's central focal point. A frontage that is not quite symmetrical can result in a development that appears aesthetically unsatisfactory.

As is noted above, whereas a typical development need only concern itself with addressing the street to its front, in your case the development must also address the footpaths on either side, and also (at least on the upper floors) the development to the rear, meaning that all four elevations must be carefully considered. The reasons for this are firstly aesthetic in that the development will be seen by people from all directions; secondly they relate to the pedestrian experience in that the Council seek to ensure that passers-by will feel welcome and at ease, rather than unwanted or excluded when they use the streets or footpaths; and thirdly they relate to the principles of designing out crime and the fear of crime (NPPF 2018 paragraph 91b) by ensuring that the footpaths on either side and the neighbouring private carpark behind are overlooked by windows that face onto them.

For these reasons I suggest that you reconsider your proposal to use the ground floor for undercroft parking because that would present a "dead" frontage to the street at ground level, and also to the paths on either side. The inclusion of some duplex apartments with some of their rooms (and windows) on the ground floor could be a solution to this problem, and it might have the added benefit of allowing some family-sized (i.e. 3-bedroom) units on the lower floors.

I appreciate that the living "green wall" of plants that you propose is well intentioned, as a way of greening and softening the site; but such a feature is expensive to install and to maintain as it must include an integrated irrigation system, and the question would arise as to who would be responsible for its long-term upkeep and the costs associated with that. My experience of these types of living green walls is that they are rarely successful, and they can result in dead foliage and exposed irrigation systems giving the walls an unsightly appearance.

Green roofs (typically consisting of sedum plants) are a less problematic way of greening a site as they do not require irrigation, and such a feature on your flat roof would be welcomed as it would benefit the development and the area by providing a habitat for insect and bird life, absorbing CO₂ and rainwater run-off, and preventing heat-absorption during hot weather.

The oriel windows that you are proposing for the upper floors on both the side elevations are features that will add visual interest and a sense of articulation to the side elevations (the privacy of neighbours could be protected by making them triangular to control the direction of the view) and the same is true of the rear balconies. The front balconies similarly add visual interest and articulation, but I suggest that they be re-designed as Juliet-balconies to protect the privacy of the private gardens and rear windows of the houses opposite (see below), given how close those would be.

When we met at the site on 17th January you mentioned that you were considering grey bricks as the main finishing material. I agree that bricks are the appropriate

choice of material, but I am not convinced that grey is the right colour for this development. I worry that it would appear out of keeping with the street scene of Coleridge Way, which is typified by yellow bricks. I appreciate that you are seeking to reference the grey bricks that are to be used at the new flatted development to the rear of your site (Allocation site H2) but it is more important to acknowledge the material that characterises the street that you are fronting. The neighbouring buildings on either side and opposite your site are all finished in yellow bricks. I suggest that you consider opting for a London Stock type of brick (such as the brick that the existing house on the site is made of) because this is slightly darker than the pale yellow bricks of the neighbouring buildings and thus it would avoid monotony of tone, but without seeming anomalous in the street-scene.

Parking, Transport and Highways

The indicative floor plan that you have submitted shows that the first, second and third floors would each have 2 x 2-bedroom flats and 1 x 1-bedroom flat, making a total of 6 x 2-bedroom flats and 3 x 1-bedroom flats (a total of 9 flats).

Policies CS25 and SADM40 relate to parking requirements, and they are linked to the Parking Standards supplementary planning document. The SPD states that the standard requirement for car parking spaces for a residential development is 1.5 spaces for a 1-bedroom dwelling or 2 spaces for a 2-bedroom dwelling, which in this case would mean a requirement of 17 spaces. However the SPD places this site in Residential Accessibility Zone 3, meaning that a discount of 50% to 100% may be applied (subject to justification and at the Council's discretion). In other words, according to the SPD, if only half the standard number of spaces were to be proposed (i.e. in this case 9 spaces) that could accord with the SPD. You are currently proposing 11 spaces to serve the 9 flats.

Local residents and Councillors sometimes worry about planning applications that propose fewer off-street parking spaces than the SPD requires because they fear that residents of the new development might find themselves unable to park within the site, and that they might instead seek to park on local streets. However in this case I consider that there is no likelihood of such a problem arising because the surrounding streets are all covered by parking restrictions. Given that the site is located just a few minutes' walk from Elstree and Borehamwood Station and close to the shops and services of the Town Centre, and being mindful of paragraph 106 of the National Planning Policy Framework 2018, my own opinion is that a development would be acceptable here with fewer than one parking space per dwelling – for instance retaining some or all of the five parking spaces that you have proposed on the forecourt, but deleting the six undercroft spaces. In that way you could provide parking spaces for disabled residents (this is only required by the SPD paragraph 2.5 for larger schemes than yours, but is never the less encouraged) and for visitors, while freeing up space on the ground floor – for instance to allow for some duplex apartments – thereby creating the active frontage and side elevations that I recommended above to benefit the street-scene.

Given the concerns that local residents are likely to have, it will be important that your planning application should be accompanied by a Transport Statement justifying why fewer parking spaces are proposed than the SPD requires, and

pointing out (perhaps with a map) that local streets for some considerable distance around this site are well protected by parking restrictions.

Secure and weatherproof bicycle parking should be included within the development, as well as adequate and convenient storage for waste and recycling bins.

Protecting the privacy of neighbours

A significant constraint on this site is the way that it relates to the neighbouring houses to the East at 17-23 Shenley Road, as the development will stand level with their private rear gardens, and with the houses opposite at 79-85 Coleridge Way, as their private rear gardens and rear windows face the front of your site.

Any side windows that face east should either be obscurely glazed and permanently fixed shut (except for parts that are at least 1.7m above the floor) to prevent overlooking of the neighbours' gardens, or else they could be designed as angled oriel windows which direct the view to the South-East. A bedroom or living room without an outlook would not provide an acceptable standard of accommodation, so the use of obscure glass would not be an acceptable solution to the problem if it were the sole window of a bedroom or living room.

The emerging supplementary planning document Planning and Design Guide Part D: Guidelines for High Quality Sustainable Development (2016 consultation draft) is not yet adopted, but it never the less carries some weight as a material planning consideration because it has been published and subjected to public consultation. Section 3b seeks to prevent unreasonable overlooking of sensitive private areas at close quarters. In particular points (h) and (i) require a separation distance of at least 20m (or in some cases up to 28m) between unobscured upper floor windows of habitable rooms where the rear windows of the neighbour are being overlooked. Given that this is a densely developed area close to the Town Centre, where properties stand in close proximity to each other, it is my opinion that the minimum requirement of 20m is the more relevant. You will need to consider the front building line of your development carefully, and I suggest that the front balconies be changed to Juliet balconies – both to reduce their proximity to the neighbouring gardens opposite, and to prevent anyone standing out on the balconies from being able to look sideways into the rear gardens of Shelley Close.

Quality of accommodation

Policy SADM3 of the Hertsmere Local Plan (Site Allocations and Development Management Policies Plan 2016) requires that new residential developments provide a good standard of accommodation.

The National Planning Policy Framework (2018) paragraph 127f and footnote 46 state that new development should provide a high standard of amenity for existing and future users, and that “policies may...make use of the nationally described space standard, where the need for an internal space standard can be justified.”

Such a justification exists in Hertsmere because the adopted *Hertsmere Local Plan, Site Allocations And Development Policies Plan* (adopted 2016) section 2.23 states

that standards of amenity in new residential development will be expected to comply with internal space standards, as set out in the *Planning and Design Guide SPD*. That SPD is an adopted supplementary planning document, but the old internal space standards that it includes have been superseded by the government's document *Technical Housing Standards: Nationally Described Internal Space Standards*, and those newer national standards are referred to in the emerging *Planning And Design Guide Part D (Guidelines for High Quality Sustainable Development)* supplementary planning document (consultation draft published in October 2016). The government's *Nationally Described Internal Space Standards* are therefore a material planning consideration.

It is worth remembering that those national standards do not only set minimum sizes for the overall floorspace of a dwelling, but also for the area of bedrooms, the minimum width of bedrooms, and for storage space. We will also expect that habitable rooms should have adequate levels of natural light and a decent outlook (which of course must not threaten the privacy of a neighbour).

Other matters

Given that the site was adjacent to a former gas-works, the question of whether the soil might be contaminated will be a material issue, which should be addressed in any planning application.

As the proposal is to provide fewer than ten dwellings, there will be no requirement to provide Affordable Housing. The Community Infrastructure Levy (CIL) will apply.

Conclusion

In my opinion, if the design were to be revised along the lines that I have suggested above, a residential scheme could be created on this site that would provide a good standard of accommodation and benefit the street-scene and the character of the area. The privacy of the neighbouring premises, and the way in which the site relates to the experience of pedestrians on the street in front and on the paths on either side of the site will be important considerations.

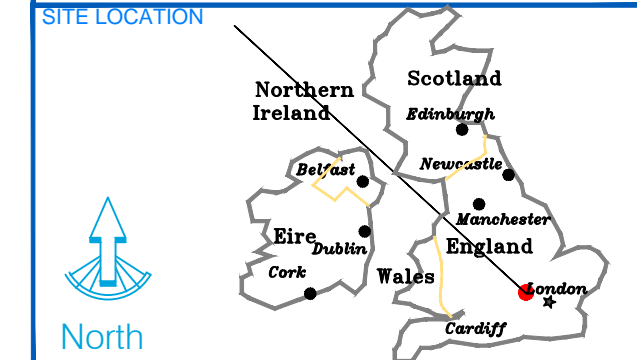
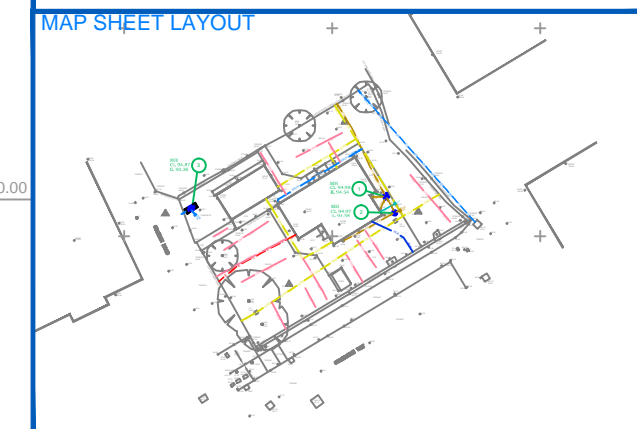
I hope that you find the comments above helpful, but please be aware that they are my own opinions only, and are not binding on the Council.

Yours sincerely



Max Sanders - Senior Planning Officer

STATION	DESCRIPTION	EASTING	NORTHING	LEVEL
STN1	Control Station	210256.039	116036.644	95.497
STN2	Control Station	210256.858	116115.403	94.952
STN3	Control Station	210268.291	116122.338	94.946
STN4	Control Station	210261.213	116131.006	94.877
STN5	Control Station	210263.970	116132.100	94.960



REVISION:

SCALE: 1:100@A1	DATE: JAN 2018
DRAWING NO: 01	DRAWN BY: KW
SURVEYED BY: KW/BKLP	CHECKED BY: KW

LEVELS RELATE TO:
To GPS DATA

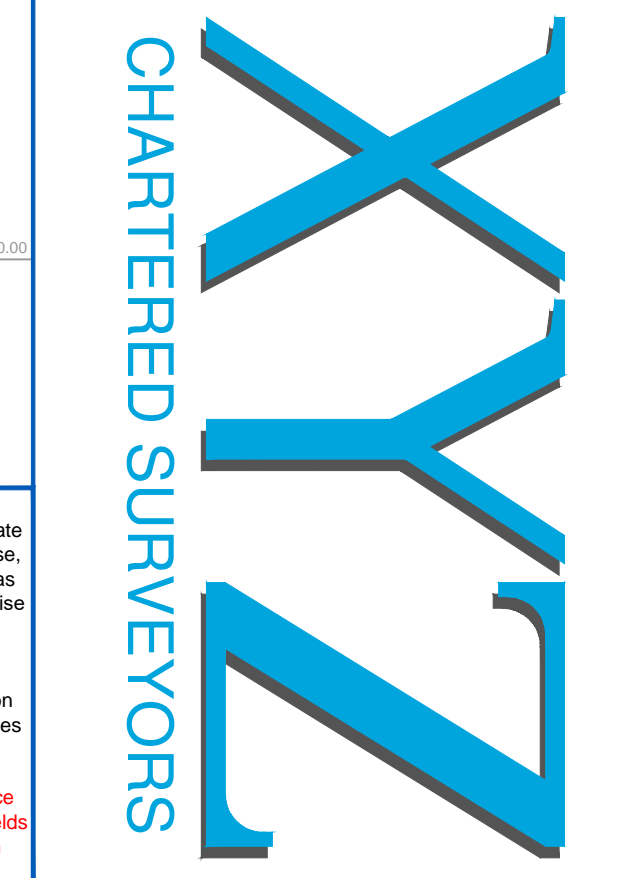
SITE ADDRESS:
GASWORKS COTTAGE
STATION ROAD
HERTFORDSHIRE
WD6 1DB

DRAWING TITLE:
UTILITY GPR SURVEY

JOB NUMBER:
2018/004

CLIENT:
HAMILTON COURT
DEVELOPMENTS LTD

**XYZ LAND SURVEYS
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TEL 020 8255 4550
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LEGEND		ON DETAIL SURVEY	
E	Electrical Cable	AC	AIR CONDITIONER
EL	Electrical Lighting Cable	BB	BELISHA BEACON
FS	Foul Sewer	BH	BORE HOLE
SW	Surface Water Drainage	BN	BRITISH TELECOM COVER
TELECOM	Telecommunications	BOX	UNIDENTIFIED BOX
W	Water Main	C-BOX	CONTROL BOX
UNDO	Undetermined Service	CL	COVER LEVEL
CM	Combined Sewer	CPOST	CONCRETE POST
ED	Empty Ducts	CPL	CONCRETE PAVING SLABS
GAS	Gas Pipe	CPS	CABLE TV COVER
Cable	Cable	EQ	ELECTRIC COVER
DP0.5	Depth 0.50 (Example)	ER	EARTH ROD
U.T.O.	UNABLE TO OPEN	FH	FIRE HYDRANT
	Circular Manhole	FP	FLAG POLE
	Square Manhole	G	GULLEY
	Foul Manhole	GV	GAS VALVE
	Surface Water Manhole	HT	HEIGHT INSPECTION COVER
	Manhole Identification Number (for use with MH condition sheets)	IC	INVERT LEVEL
	CCTV Report Number (for use with CCTV Report)	IP	INSPECTION PLATE
		LB	LETTER BOX
		LC	LAMP COLUMN
		LT	LIGHT MANHOLE
		MKR	MARKER
		MLP	MILEPOST MARKER
		MPT	MOORING POST
		NB	NOTICE BOARD
		OH	OVERHEAD
		PB	POST BOX
		PL	PAVEMENT LIGHT
		PP	PETROL PUMP
		RE	RODDING EYE
		RNP	ROAD NAME PLATE
		RS	ROAD SIGN
		RWP	RAIN WATER PIPE
		SB	SAND / GRIT BIN
		SP	SIGN POST
		SVP	SOIL VENT PIPE
		TGB	TELEPHONE CALL BOX
		TH	TRIAL HOLE
		TI	TOP OF FENCE LEVEL
		TWALL	TOP OF WALL LEVEL
		TL	TRAFFIC LIGHT
		TLCB	TRAFFIC LIGHT CONTROL BOX
		TP	TELEGRAPH POLE
		UTL	UNABLE TO LIFT
		VP	VENT PIPE
		VR	VAPOUR RECOVERY
		WPOST	WOODEN POST
		WV	WATER VALVE

XYZ LAND SURVEYS, Ground Penetrating Radar Disclaimer

The survey aims to map all existing utilities and sub surface structures and provide information with respect to pipe size, material type and drainage connectivity. However GPR surveying is limited by the following guidelines and it may not be possible to accurately survey, define and locate all services and sub surface features.

- Locational accuracy is determined by referring to the manufacturers guidelines for the detectors used.
- Existing record information showing underground services is often incomplete and unknown accuracy, therefore it should be regarded only as an indication.
- In ideal conditions these spatial accuracies for the underground utilities located and mapped are +/-5% for the RD4000 and +/-10% of depth for the GPR system. Variations within the subsurface may, however, alter this estimated accuracy.
- It is not always possible to trace the entire length of each underground service.

Although all reasonable steps have been taken to locate all features, there is no guarantee that all will be shown on the drawing, as some above ground features may have obstructed the survey.

GPR surveying operates best within high resistivity material. Clay overburden can impair GPR surveying.

Due to the attenuation of the radar signal with depth, resolution is restricted, making identification of anomalies difficult with increasing depth.

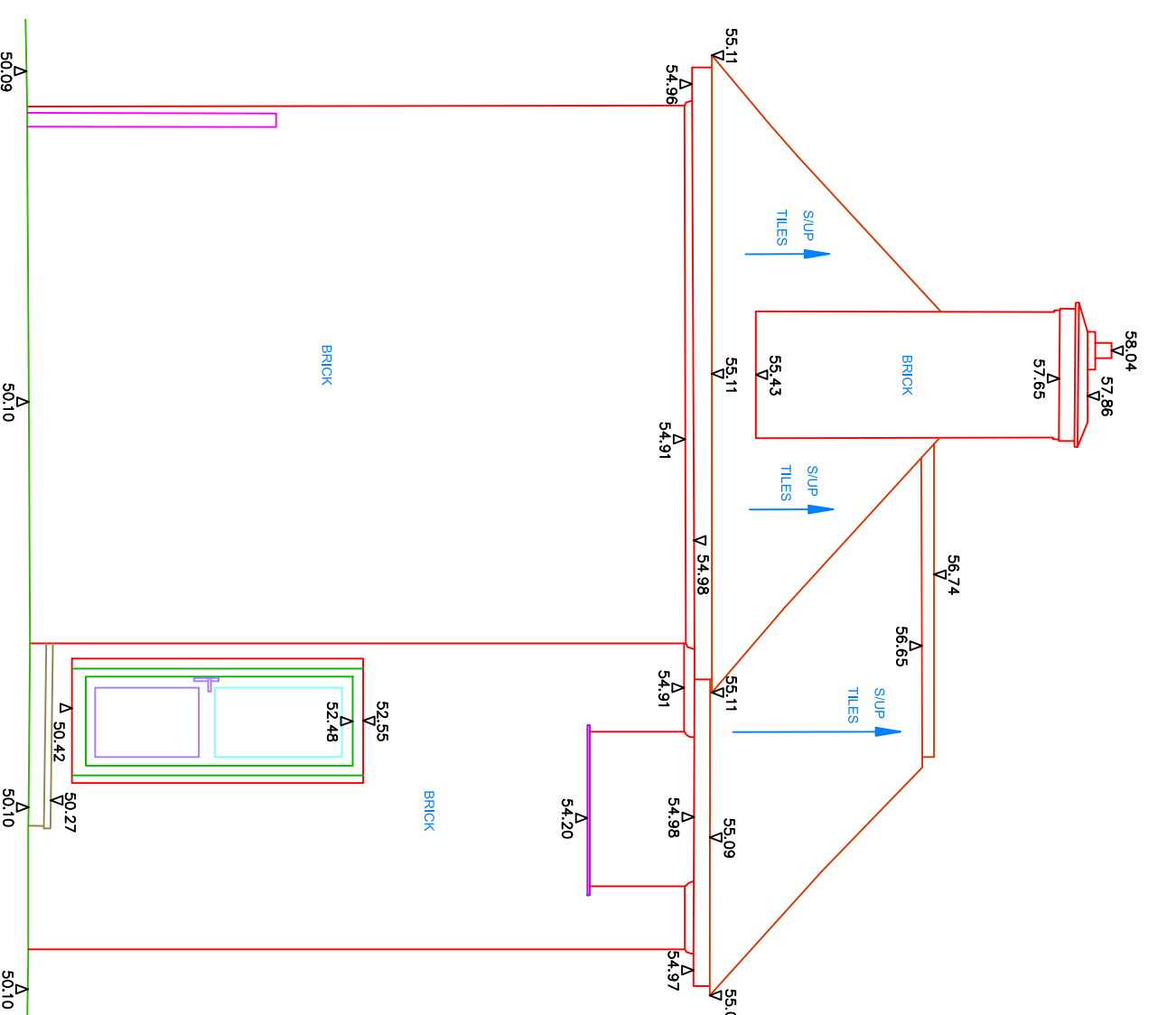
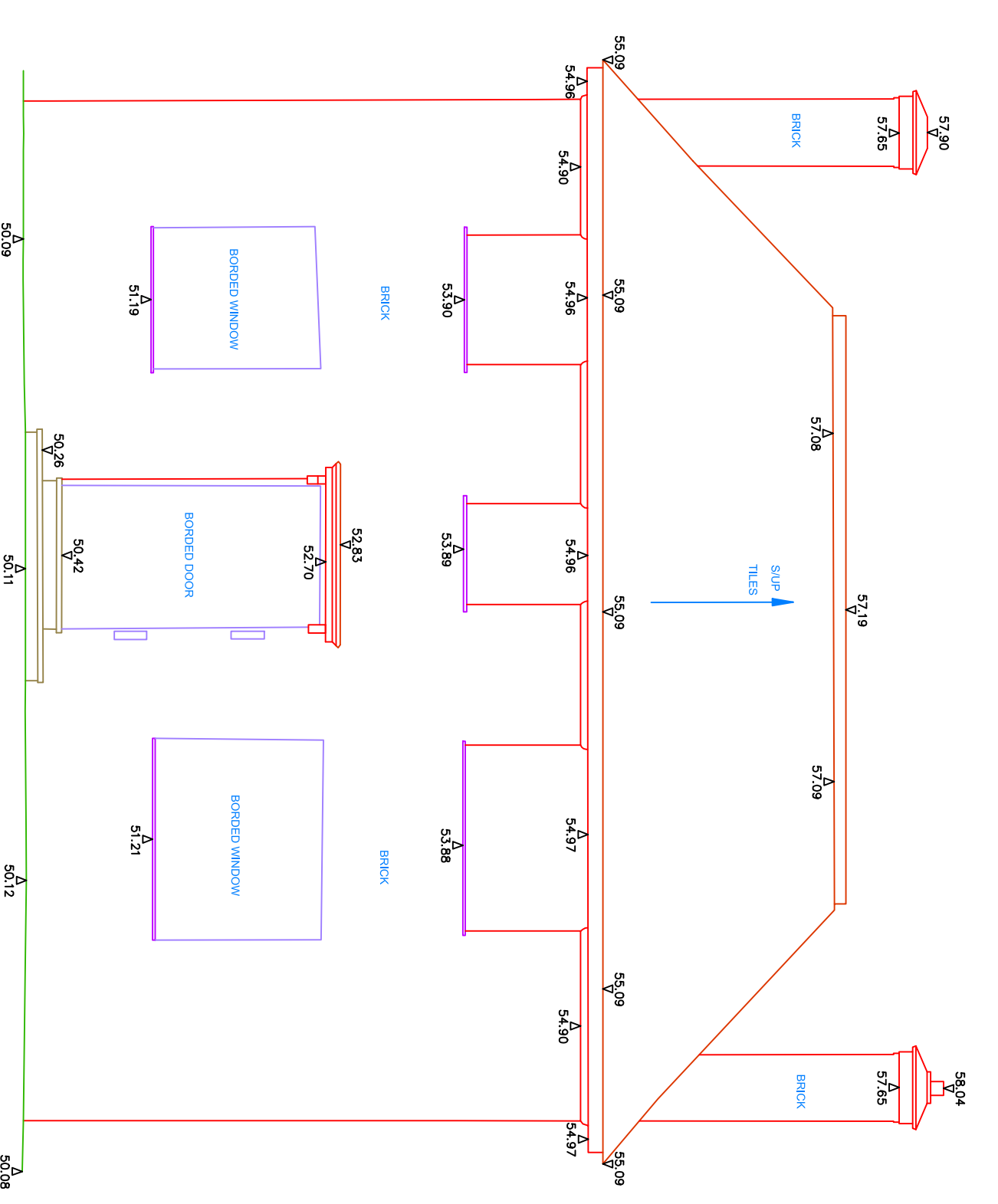
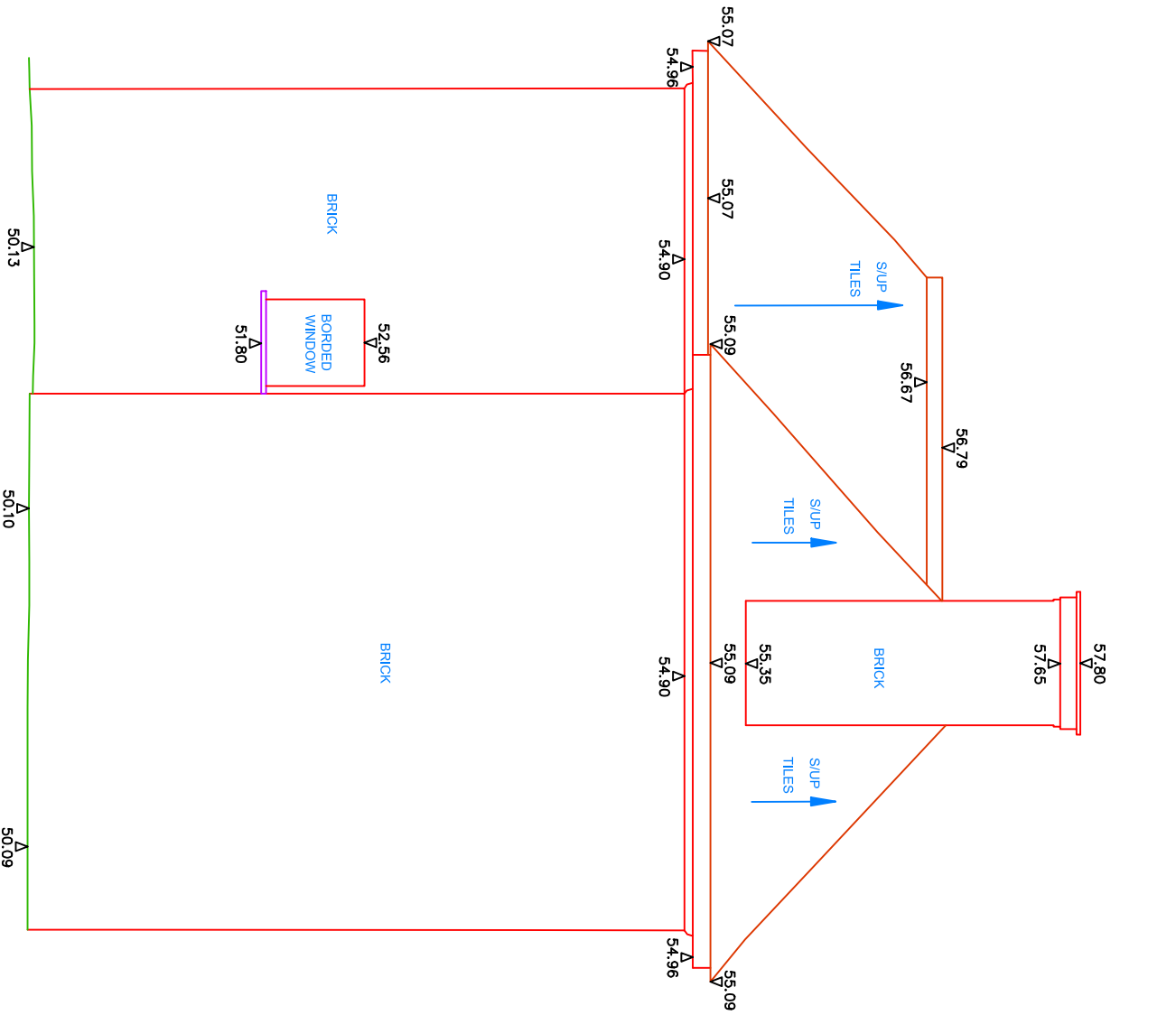
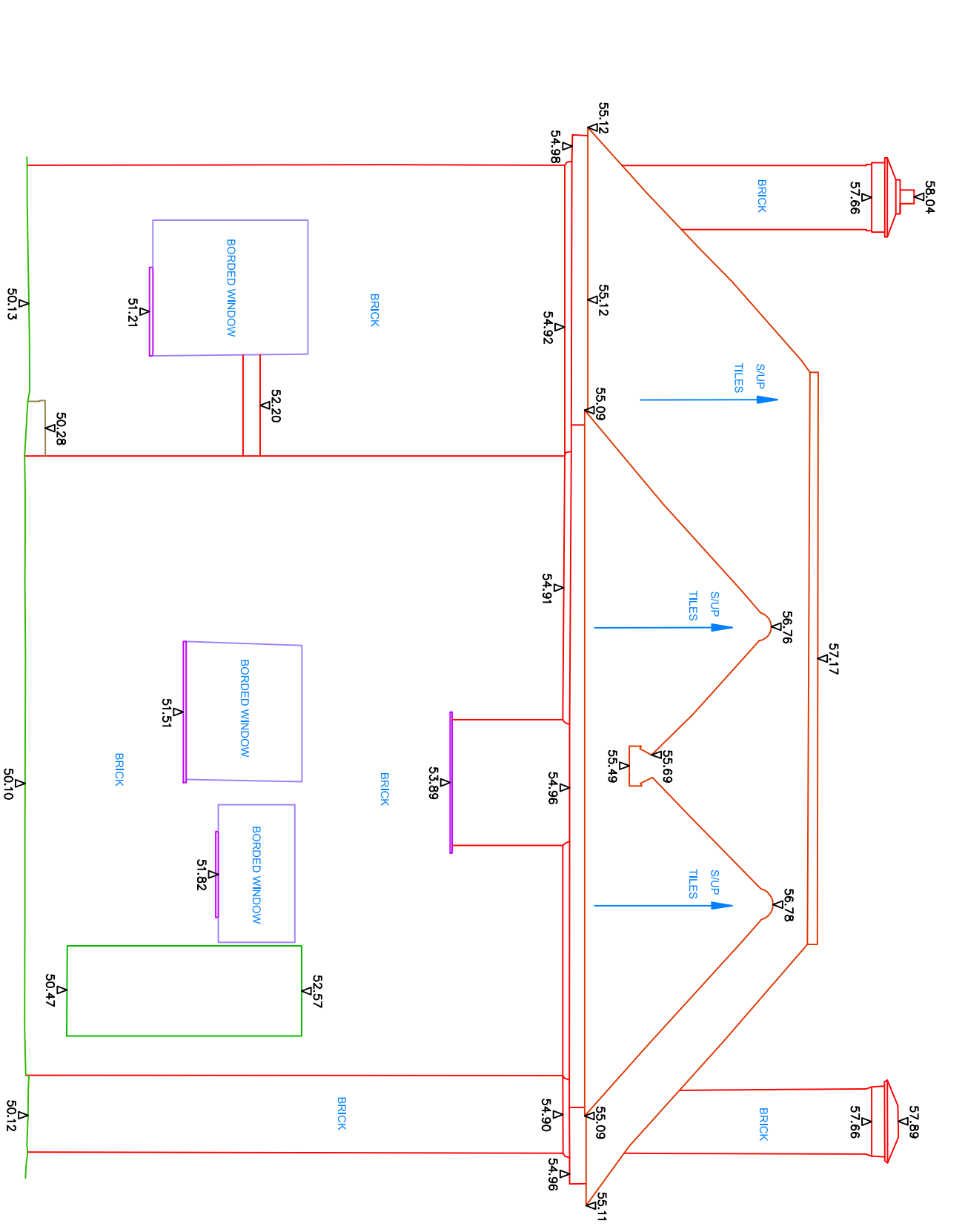
The depth penetration and quality of the data depends on the ground conditions on site. Poor data may be a result of areas with high conductivity. Also, high reflective materials close to the surface. E.g. Rebar may hide deeper anomalies.

Unless otherwise stated, all services and sub surface structures shown on these drawings have been surveyed using approved detectors and the connections between manholes, if not traced, are assumed to be direct.

Where supplied prior to survey commencement, we will use utility provider's information as a guide for location purposes. However, should we not be able to locate those guided services, we shall not be held responsible for the accuracy, or otherwise, of the location of that service, as issued by the utility provider. These will be shown as "Taken From Records" on the drawing and we are not liable for any loss that may arise due to the lack of accuracy in the guided information.

Plan accuracies of the order of +/- 150mm may be achieved but this figure will depend on the depth of the service below ground level. Where similar services run on close proximity, separation may be impossible. Successful tracing of non metallic pipes may be limited.

No guarantee can be given that all services have been shown. Underground service tracing is not an exact science - Errors and omissions can occur, due to electrical fields and localised ground conditions. We advise that excavation and further investigation may be required before starting work on site.

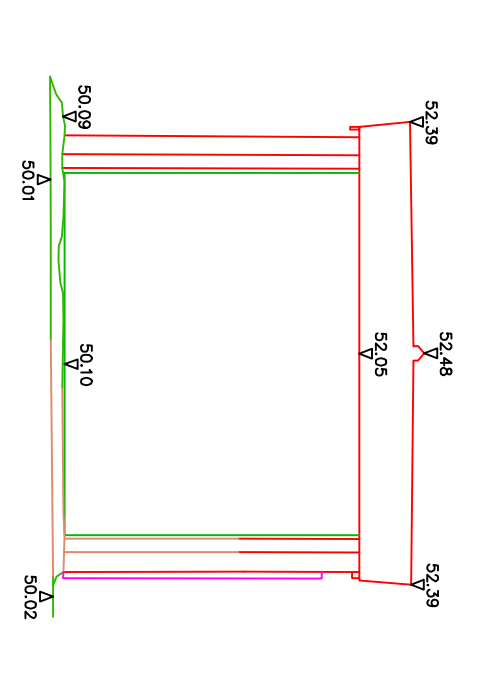
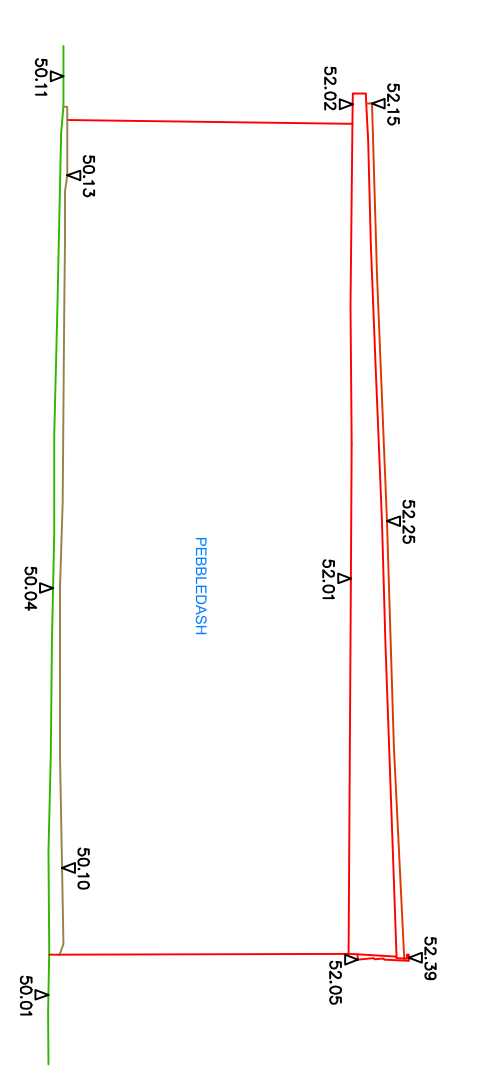
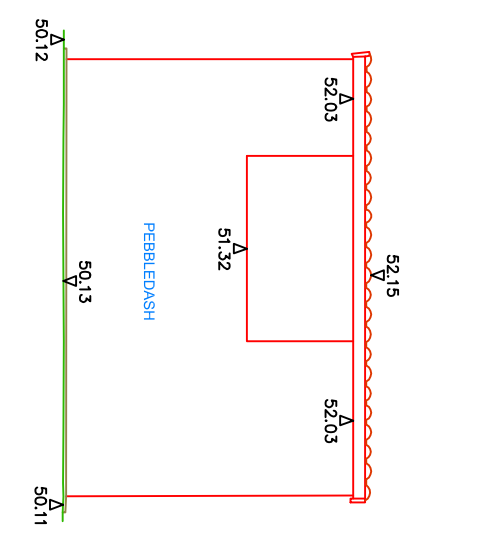
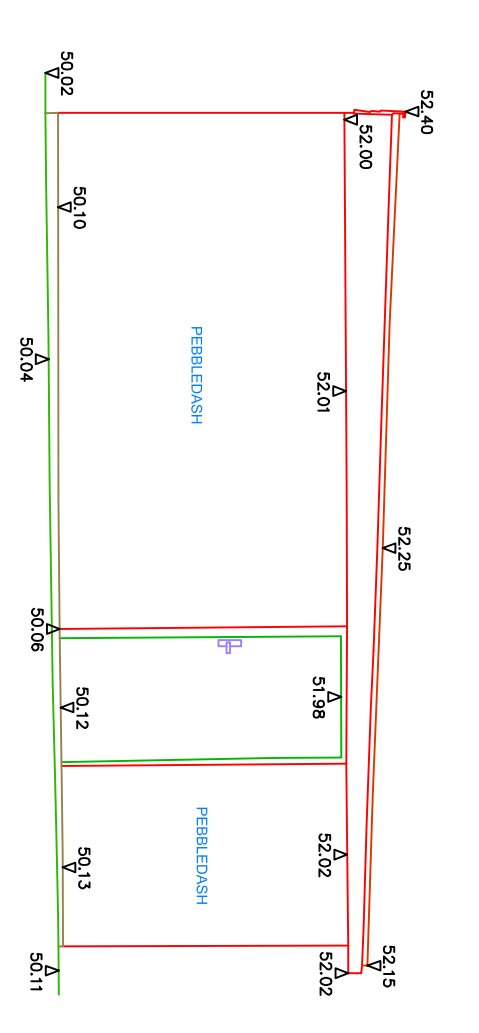


DATUM 50.00m
ELEVATION 1

DATUM 50.00m
ELEVATION 2

DATUM 50.00m
ELEVATION 3

DATUM 50.00m
ELEVATION 4

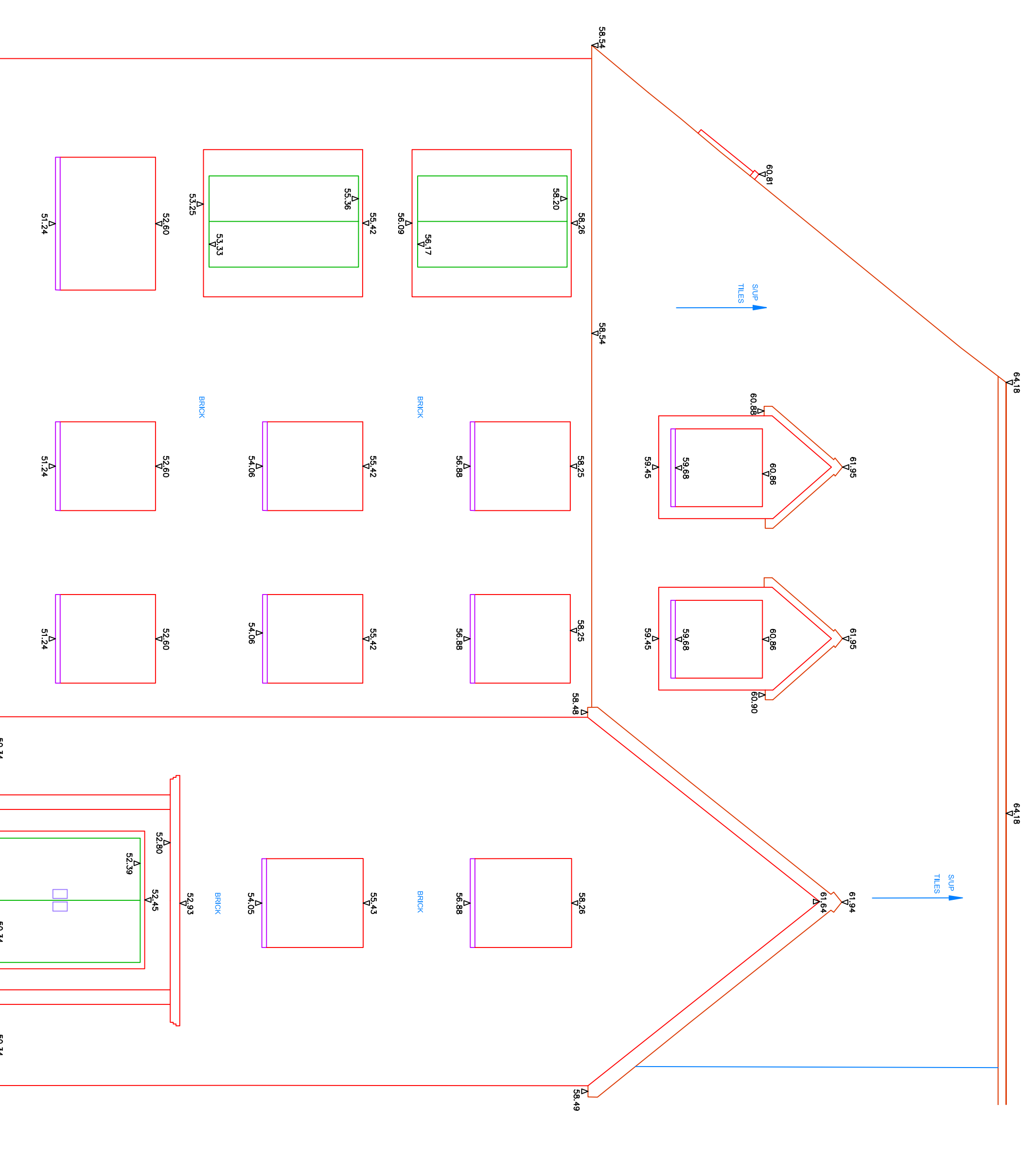
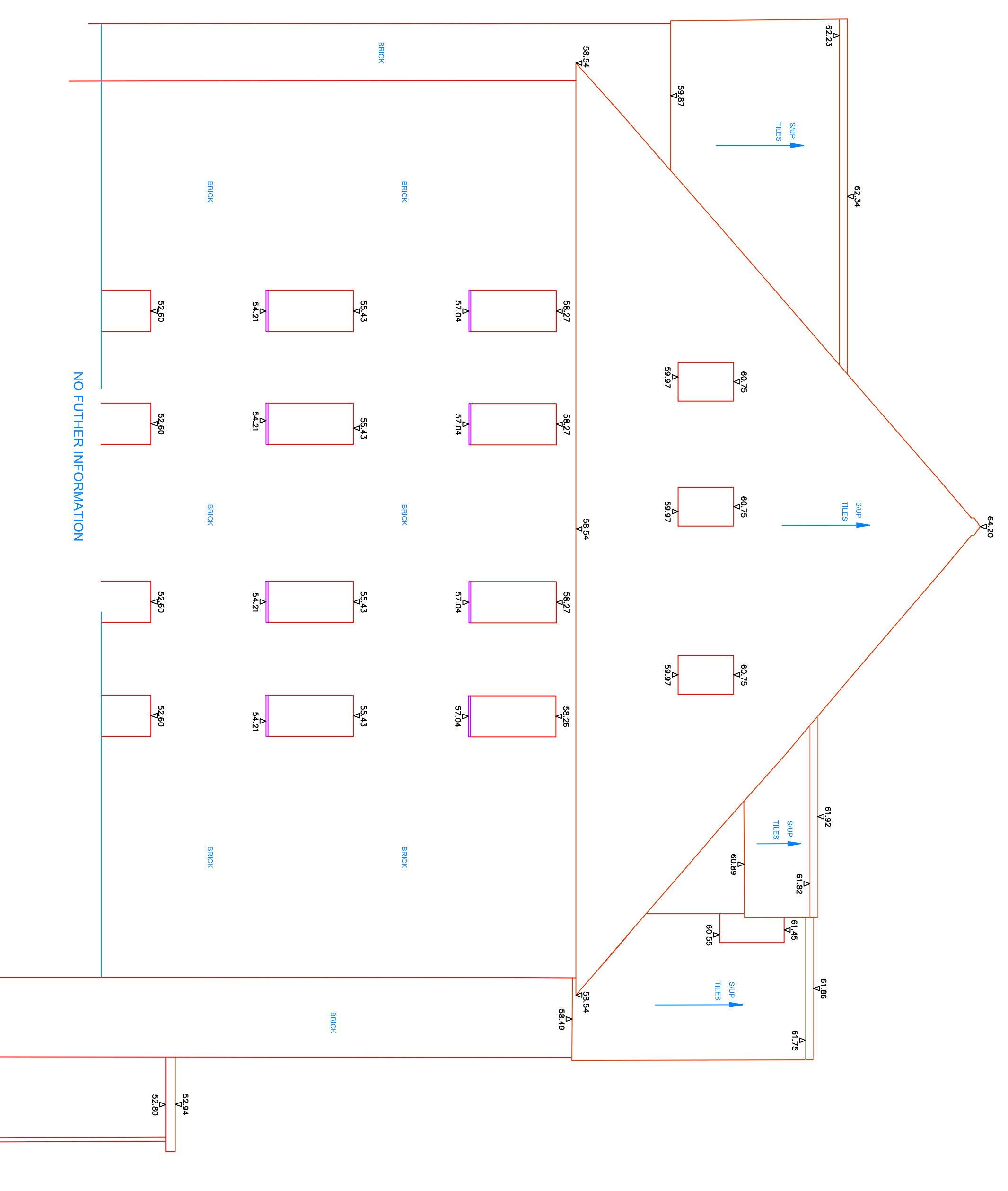
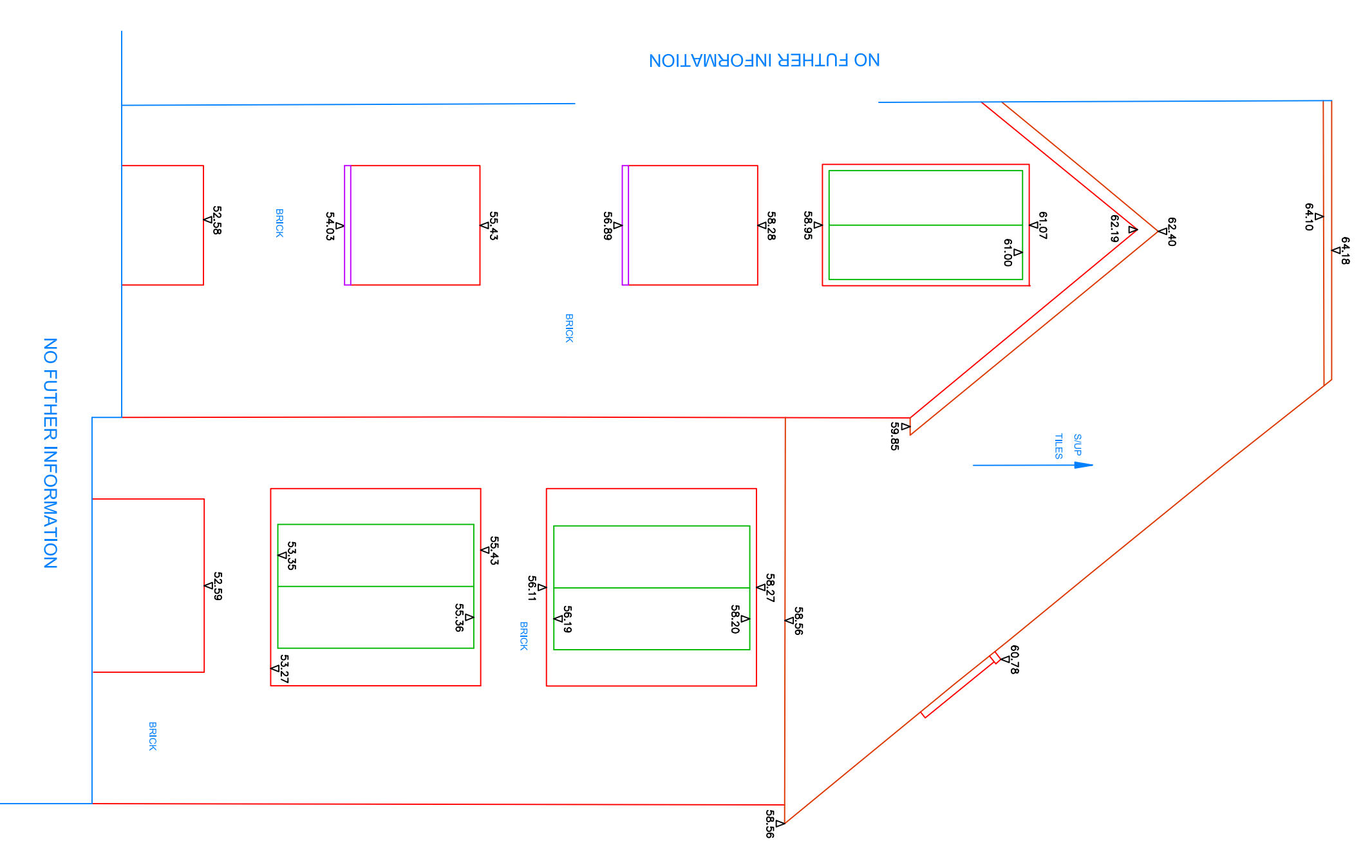


DATUM 50.00m
ELEVATION 5

DATUM 50.00m
ELEVATION 6

DATUM 50.00m
ELEVATION 7

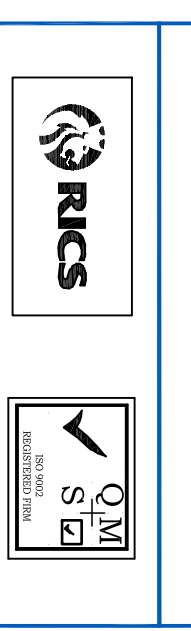
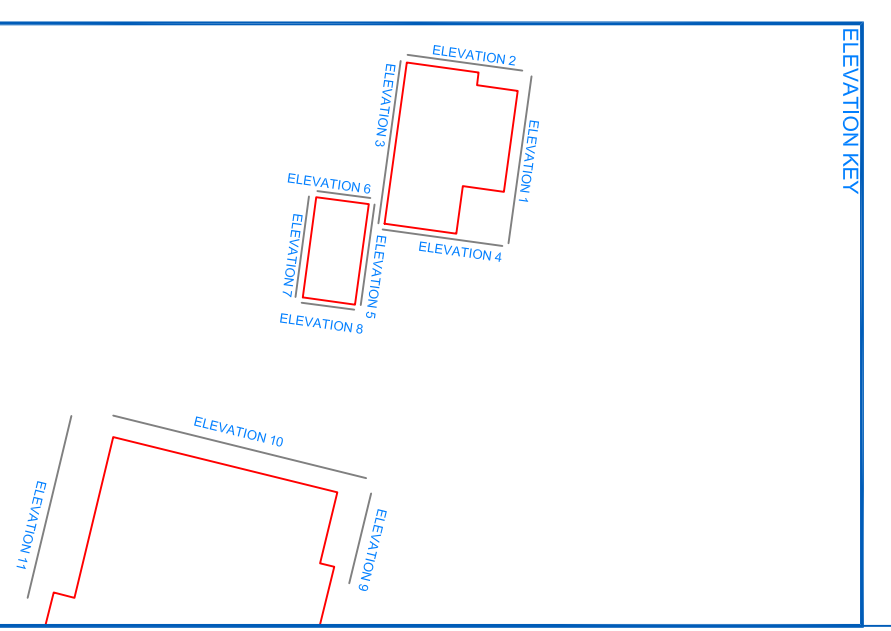
DATUM 50.00m
ELEVATION 8



DATUM 50.00m
ELEVATION 9

DATUM 50.00m
ELEVATION 9

DATUM 50.00m
ELEVATION 10



REVISION:

DATE:	15/01/20	BY:	JAWAD/018
REVISION:	02	BY:	SV
DATE:		BY:	
DATE:		BY:	

LEVELS RELATE TO:
OS DATA

SITE ADDRESS:
GASWORKS COTTAGE
STATION ROAD
HERTFORDSHIRE
WD6 1DB

DRAWING TITLE:
ELEVATIONS
JOB NUMBER:
2018/002

CLIENT:
HAMILTON COURT
DEVELOPMENTS LTD.

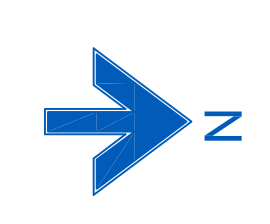
XYZ LAND SURVEYS
CHARTERED SURVEYORS
HEAD OFFICE: 100, MARKET ROAD
TRENTH CLAYDON, LUTON, MK17 9JH
E-MAIL: XYZ@XYZSURVEYS.CO.UK
TEL: 01525 232323



COORDINATE TABLE				
STATION	DESCRIPTION	EASTING	NORTHING	LEVEL
ST1	Corner Station	101000.00	100000.00	10.00
ST2	Corner Station	101000.00	100000.00	10.00
ST3	Corner Station	101000.00	100000.00	10.00
ST4	Corner Station	101000.00	100000.00	10.00
ST5	Corner Station	101000.00	100000.00	10.00



SYMBOL	DESCRIPTION
(Symbol)	CONCRETE
(Symbol)	GRASS
(Symbol)	EXISTING FENCE
(Symbol)	NEW FENCE
(Symbol)	LEVEL POINT
(Symbol)	STATION
(Symbol)	BOUNDARY
(Symbol)	ROAD
(Symbol)	UTILITY
(Symbol)	OBSTACLE
(Symbol)	PLANT
(Symbol)	WATER
(Symbol)	TOPOGRAPHY
(Symbol)	LEVEL RELATE TO: TO GPS DATA



REVISION:

NO.	DATE	DESCRIPTION
1	01/2018	ISSUED FOR TENDERS
2	01/2018	ISSUED FOR TENDERS
3	01/2018	ISSUED FOR TENDERS
4	01/2018	ISSUED FOR TENDERS
5	01/2018	ISSUED FOR TENDERS
6	01/2018	ISSUED FOR TENDERS
7	01/2018	ISSUED FOR TENDERS
8	01/2018	ISSUED FOR TENDERS
9	01/2018	ISSUED FOR TENDERS
10	01/2018	ISSUED FOR TENDERS

LEVELS RELATE TO:

To GPS DATA

SITE ADDRESS:

GASSWORKS COTTAGE
STATION ROAD
HERTFORDSHIRE
WD6 1DB

DRAWING TITLE:

SITE SURVEY

JOB NUMBER:

2018/002

CLIENT:

HAMILTON COURT
DEVELOPMENTS LTD.

XYZ LAND SURVEYS

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WHITE
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